

**REPORT OF THE
VIRGINIA COMMONWEALTH
UNIVERSITY/TRANSPORTATION SAFETY
TRAINING CENTER STUDYING**

**SPECTATOR SAFETY AT
CERTAIN MUD RACING
COMPETITIONS**

**TO THE GOVERNOR AND
THE GENERAL ASSEMBLY OF VIRGINIA**



HOUSE DOCUMENT NO. 29

**COMMONWEALTH OF VIRGINIA
RICHMOND
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Virginia Commonwealth University

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CENTER FOR PUBLIC POLICY

TRANSPORTATION SAFETY
TRAINING CENTER
921 WEST FRANKLIN STREET
P.O. BOX 843023
RICHMOND, VIRGINIA 23284-3023

804 828-6235
FAX: 804 828-1848
TDD: 1-800 828-1120

TO: The Honorable George Allen, Governor of Virginia

and

Members of the Virginia General Assembly

The report contained herein has been prepared pursuant to House Joint Resolution Number 87.

This report, prepared by the Transportation Safety Training Center, studied spectator safety at certain mud racing competitions in the Commonwealth.

Respectfully submitted,

A handwritten signature in black ink, which appears to read "Robert J. Breitenbach". The signature is fluid and cursive, with a large, sweeping flourish at the end.

Robert J. Breitenbach
Director

PREFACE

House Joint Resolution Number 87, passed by the 1996 session of the General Assembly, requested Virginia Commonwealth University's Transportation Safety Training Center (TSTC) to study spectator safety at certain racing competitions. This request stemmed from an incident at a motor sports park in Cumberland County in which a specially designed mud dragster, out of control, vaulted a guardrail and crashed into a spectator area killing three people and injuring six others. With this resolution, the Virginia General Assembly hoped to determine what, if any, safety requirements were necessary to ensure the safety of spectators at such events. While no single agency of state government is charged with the unified and comprehensive responsibility for the safety and protection of spectators at similar racing competitions, TSTC was deemed to have expertise in the areas of transportation safety and accident investigation that would allow them to determine the probable factors contributing to this tragic fatal crash and offer, if applicable, any safety recommendations that would prevent similar occurrences.

The TSTC has at its disposal the state's Multidisciplinary Crash Investigation Team (CIT) which has undertaken this study. Team members include a highway engineer, a Virginia state trooper (assigned to the Team on a special assignment from the Department of State Police), a psychologist and when appropriate, consultants from other disciplines. The Team used its expertise in the investigation of the circumstances surrounding this fatal

crash and the gathering of pertinent information concerning spectator safety at similar racing events.

As indicated in the resolution, the study centered solely on events involving “monster cars” and “mud dragsters.” The Team was to consider what steps are currently in place to ensure spectator safety, determine if any present state regulations apply, and then determine whether some regulation of these events and the facilities at which they are held would reduce the risk of injury to spectators. As mandated in the resolution, those speedways and race tracks holding races sanctioned by the National Association for Stock Car Auto Racing (NASCAR) and the National Hot Rod Association were excluded.

During its investigation, the CIT contacted the Board of Housing and Community Development, the Virginia Municipal League, the Virginia Association of Counties, Indianapolis Motor Speedway, the National Mud Racing Organization (NMRO), the Eastern Mud Racing Association (EMRA), the recently formed Atlantic Mud Racing Association, the now-defunct Mid-Atlantic Four-Wheel Drive Association, the State Corporation Commission, several insurance companies that provide special coverage on these types of events, and the New Jersey Division of State Police. Bodacious Motor Sports Park officials and two other mud racing track officials were interviewed and their facilities were examined by the CIT. Two counties’ fairgrounds that allow mud bog racing were also contacted.

The following pages address the findings and conclusions, as set forth under the guidelines of this study, and are offered by the TSTC for consideration.

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REPORT SUMMARY

1. On May 6, 1995, at the Bodacious Motor Sports Park in Cumberland County, a custom-built car racing on a mud drag strip crashed into a spectator area. As a result of this crash, which killed three people and injured six others, the Virginia Senate and House of Delegates agreed upon House Joint Resolution Number 87 on March 6th and 7th, 1996, respectively.
2. House Joint Resolution Number 87 requested Virginia Commonwealth University's TSTC to study spectator safety at certain racing competitions. Specifically named in the resolution were vehicles referred to as "monster cars" and "mud dragsters." The resolution also read that the Center shall consider whether some form of regulation of these events and facilities is in the interest of reducing the risk of injury to spectators.
3. Based on the findings surrounding the fatal crash in Cumberland County, it was determined that the dragster's left rear tire became partially separated from its wheel while it was accelerating during a timed event. As a result of unequal acceleration forces exerted upon the rear wheels, the dragster became unstable and headed toward the guardrail which separates the mud track race lane from a spectator area. At speeds approaching 100 mph (and within only three seconds), the dragster became airborne, vaulted the railing, crashed through a metal fence and landed

inside the spectator area. As it bounced through this area, it struck several spectators and parked vehicles before coming to rest.

4. At present, the Commonwealth has no safety requirements or liability insurance mandates that govern the safety of spectators or participants at these types of events. There are, however, safety guidelines established and used by the race track's sanctioning body, the track itself and/or the insurance companies which cover these events.
5. Due to the fluctuation in popularity of these types of events in Virginia, the number of organized races vary, but generally there are about a dozen annually. It appears that mud racing (also known as mud drags, mud slings, and mud bogs) has evolved from drag strips to sand drags and then to mud drags. The dragsters used are, in general, highly sophisticated vehicles which are specially designed for rapid acceleration over this type of terrain.
6. There are a number of sanctioning bodies involved with mud racing which have established safety guidelines for the vehicles, track, and spectator areas. These guidelines, which have evolved over the years from other forms of motor sports racing, are typically used at events such as Bodacious. Most spectator safety guidelines include the use of barriers and/or fences separating spectators from the racing vehicles, the positioning of spectators away from the starting line and shut down areas, and keeping spectators at a required distance of about 30 feet from the track during racing events. Not all mud racing events are sanctioned.

7. Since mud racing is a highly specialized sport and not widely practiced when compared to typical drag racing or oval track racing, only a few insurance companies offer liability coverage for these events. Of those doing business in Virginia, all require that the scheduled mud racing competition meet minimum safety guidelines -- similar to the typical mud racing organization requirements -- enhancing participant and spectator safety. As can best be determined, on the date of the fatal crash at Bodatious, the mud race track facility met or exceeded the minimum safety guidelines established by its sanctioning racing body and its insurance carrier. While there is no requirement that these race tracks carry insurance, the five race track officials interviewed for this report indicated their tracks were insured.
8. As with any motor sports racing competition, certain risks and dangers to the participants and spectators are realized. According to the insurance carriers, mud racing participants, and racing enthusiasts, a mishap such as the one which occurred at the Bodatious track is a "freak occurrence". Nationally, according to insurance claim adjusters, such incidents are extremely infrequent. As can best be determined, no other such mishap has ever occurred in Virginia involving a mud dragster.
9. On the day after the fatal crash at Bodatious, before the event's second day of racing competition began, several improvements were made by park officials. The spectator area on the south side of the mud strip (where the dragster entered the

spectator area) was closed off and prohibited for pedestrians and vehicles.

Additional improvements to further separate and protect spectators at this racing facility included the placement of a barrier consisting of large “earth mover” equipment tires (in addition to the existing guardrail barrier and fence line) to separate the drag strip from the spectators on the north side of the facility. To create a more protective barrier between the spectators and dragsters, the already existing hillside was cut to create a near 90 degree, 15 foot high cliff, minimizing any chance that the dragsters could enter the north side spectator area.

10. At present, most of the mud racing events in Virginia are sanctioned by one of several mud racing organizations or associations. Different sanctioning bodies may have different racing guidelines and standards for spectator locations. It is up to the event promoter to use whichever sanctioning body he chooses, or he may choose not to use a sanctioning body at all. Additionally, under present Virginia State Corporation Commission policy, these mud racing events are not required to be insured or licensed. It appears that the decision of whether or not to require insurance coverage and/or a special permit of operation rests with each individual county or locality where such events are staged.
11. The State of New Jersey has the most stringent state-sanctioned guidelines for mud racing competition (as well as other forms of motor racing). The many requirements set forth in the New Jersey Code include licensing and insurance regulations, building inspection forms, monthly race track reports, auto racing

accident report forms, and numerous guidelines specific to the vehicle, driver, facility and spectator safety areas. Although the Commonwealth of Virginia does not by statute have any of these requirements, the five race tracks reviewed for this report comply with all or nearly all of these guidelines.

12. It would certainly be possible to further increase the safety of spectators by moving them further back than the prescribed distances or having stronger, higher and more substantial barriers and/or fencing separating them from the event. Such attempts may be cost prohibitive to the promoter and spectator and may diminish the appeal to these types of racing enthusiasts. Given the rarity of such tragic events, it should be considered that existing safety requirements -- if complied with as outlined by the various mud racing sanctioning bodies and/or insurance companies -- may be adequate and reasonable. Any regulations or safety enhancements should be designed, engineered, and constructed by the industry (e.g. mud racing sanctioning bodies, promoters and insurance carriers). See Appendix for spectator safety guidelines.

BACKGROUND

Mud racing in Virginia, as in other states, is an ever-evolving form of racing which involves four-wheel drive vehicles. Its appeal reflects the popularity of general auto racing and has increased with the popularity of four-wheel drive light trucks and utility vehicles. Beginning as early as 1972, four-wheel drive enthusiasts and clubs began forming throughout Virginia. These clubs started sponsoring and promoting off-road racing/competitions and shows. One of the Commonwealth's earliest and largest such club is the Old Dominion Four-Wheel Drive Club of Richmond. The goals of the non-profit, non-political organization are to "promote four-wheeling, promote camping, provide social, educational and recreational activities for their members and others, both on and off the road. In all activities, club members strive to abide by any rules or restrictions that the landowner may impose, including good conservation practices and protection of natural resources." As with any organized club, rules, by-laws, and regulations were passed and dues were imposed.

In 1974, Old Dominion Four-Wheel Drive Club first became involved in organized, off-road racing. Competition consisted of a motorcross track, a dirt drag strip and a hill climb. With popularity growing, Old Dominion joined a larger and more encompassing organization, the East Coast Four-Wheel Drive Association. This organization, comprised of four-wheel drive enthusiasts, was also a promoter of four-wheeling and four-wheel drive

racing. Some club members joined the Virginia Wildlife Federation to assist the State of Virginia in promoting four-wheeling and to keep trails open in Virginia's forests and beaches. They assisted in clean-up details and bridge/fire-trail restorations throughout the Commonwealth.

On April 24, 1976, after club members cleared about 180 acres of land in Cumberland County near Cartersville, and designed competition racing areas (as well as camping, spectator areas, toilet and concession areas), "Bodacious" Motor Sports Park was first opened to spectators. Timed events including dirt drags, sand drags, a mud bog and a hill climb were staged. Over the coming years, different events were held at the park which reflected the diverse popularity of such events, both nationwide and across Virginia. Although all-terrain vehicles were used in some racing competitions, no motorcycle racing was ever staged at Bodacious. Monster cars or monster trucks likewise were not staged at this sports event.

As the popularity of off-road racing grew, club memberships increased, as did racing competitions and spectators at these events. In an attempt to establish racing class rules and track dimensions, as well as safety guidelines for dragsters and spectators, the Mid-Atlantic Four-Wheel Drive Association (founded in 1975 and comprised of five four-wheel drive clubs in Virginia, West Virginia and North Carolina) published an all-encompassing handbook. In 1978, all Mid-Atlantic sanctioned events were being held on Mid-Atlantic approved tracks. By 1986, the Mid-Atlantic Four-Wheel Drive Association,

Inc., was comprised of over 30 formally organized four-wheel drive clubs in the three above mentioned states.

By the early 1990's, the Mid-Atlantic Four-Wheel Drive Association disbanded due to various reasons. However, many of the individual clubs comprising that Association continued to operate. Bodacious Motor Sports Park likewise continued to sponsor competition races which perhaps reached its largest spectator attendance on May 6, 1995, the date of the fatal crash.

These types of racing events have experienced spurts of high and low popularity, depending on the racing styles and trends of the day. Likewise, since these events are held outdoors during the spring, summer and fall months, they are naturally weather driven. By 1989, ten Mid-Atlantic sanctioned events were held across Virginia between mid-April and mid-October. During this time, several other non-sanctioned events were also held. While

the track widths and lengths are usually standardized, the diversity of the parks and placement of the spectators are generally under the guidelines used by the sanctioning body and/or insurance companies. While most of these events are surrounded by campgrounds and are staged over two days of competition, some are held at county fairgrounds where fixed bleacher-type spectator seating is installed. Some mud racing events, however, consist of spectator seating on the grounds and not in fixed and raised bleachers.

CRASH CIRCUMSTANCES

On Saturday afternoon, May 6, 1995, at the Bodatious Motor Sports Park in Cumberland County, a custom built 4X4 mud dragster became out-of-control during a competition run and crashed into a group of spectators positioned beyond the finish line. The collision resulted in the deaths of three persons and injuries to six others. The mud drag race event was the first of the season and was scheduled to run for two days over the weekend. The park, in operation since 1976, was located on private property in a rural section of the county and consisted of steeply rolling hillsides. This type of "mud bog" or "mud drag" racing had been held at the park since 1986. On a typical racing day, several thousand spectators would watch up to 100 races per day. The sports park operates about four similar events annually and this was the first time that a spectator had ever been injured from a race car at this facility.

The Motor Sports Park consisted of campgrounds, a race car holding/maintenance area, a motorcross course (not used during this competition), parking lots, temporary rest rooms, designated spectator areas and the mud drag strip. The mud drag strip, as sanctioned by the National Mud Racing Organization (NMRO) consisted of two, 40 foot wide by 200 foot long competition drag strips filled with fresh mud ranging in depth from six to twelve inches. A center installed steel guardrail system separated the two drag strips. Both sides of the two drag strip lanes, which run generally east to west, had steel

“w-beam” guardrail systems which were about 27 inches above the ground. However, due to the ever-changing surface on the drag strips after each race was run, the guardrail height constantly changed. The guardrail barriers were installed to separate the dragsters from one another during the competition while also separating the cars from the spectators during an erratic or out-of-control maneuver. Located on the north side of the strip beyond the guardrail was thin wire fencing about six feet tall and 25 feet from the guardrail. About 20 feet behind this fence was a two-story, wooden control tower which housed the timers, judges, photographers and electronic equipment controlling the races. Only race-sanctioned personnel were authorized to be in this vicinity. The tower is situated in a draw located in front of and between two natural hillsides. The overwhelming majority of the spectators were positioned several hundred feet away on these grassy hillsides overlooking the drag strip. The concession stands, restrooms, parking areas, and campgrounds for the non-participants were also on the hillsides in these areas. These spectators were protected by the natural steep hillside embankment, a second guardrail system located west of the finish line, the metal fencing, and strategically parked vehicles positioned between the tower and the raised embankment.

A much smaller group of spectators (estimated at 150 people) and parked emergency/official use vehicles and attendants were positioned on the south side of the facility parallel to the strip. Adjacent to the southernmost guardrail barrier along the outside edge of the drag strip lane, was a 32 foot wide dirt service road used as a return lane by the dragsters after each competition. Thin wire fencing, similar in design to the one on

the north side of the strip, was located beside the service road to separate pedestrian traffic from vehicular traffic. The majority of this smaller group of spectators was positioned near the mid-section of the strip about 20 feet beyond this wire fencing. These persons were located about 50 feet south of the nearest guardrail barrier paralleling the drag strip and about 100 feet west of the finish line. It was at this location that the mud dragster entered the crowd and came to a stop. All of the casualties were positioned in this area.

At approximately 4 p.m. (and during the 5th run of the afternoon's competition), the involved dragster approached the starting line. The custom built, four-wheel drive dragster was equipped with a 1949 Fiat car body attached to a tubular metal frame. The large front-mounted heavy duty engine, capable of achieving nearly 2000 horsepower, was fueled by 100% alcohol for quick acceleration. The car's four large truck tires had a wide profile and were designed to carry only six pounds per square inch (psi) of air pressure (which is typical for these type of dragsters). As the 2500 pound dragster accelerated off the line with all four wheels pulling, it was observed that near the 100 foot point or midway through its race, the car began to fishtail. The car began to travel diagonally against the uneven tire ruts made by previous dragsters. The car's front end lifted off the ground and while still on an angle toward the southern guardrail barrier, the left rear tire began to partially separate from its wheel. It is probable that during the sudden acceleration of the car while the tires were achieving a spinning but lateral movement, the left rear tire's soft sidewall flexed between the uneven mud surface and wheel, causing a sudden air escape. This loss of air caused an unbalanced acceleration/traction on the rear of the vehicle thus

forcing the right wheels to bite into the mud more so than those on the left side. The mud dragster, while still accelerating, was rendered out-of-control.

Near the finish line, the dragster, now at speeds of about 100 mph and only about three seconds from the starting line, struck the left guardrail at a sharp 20 degree angle, mounted the rail and became airborne. As it clipped the rail, the left rear wheel and tire became completely detached from the axle. The dragster traveled airborne over 25 feet before landing and crashing through the wire fencing. It then entered the spectator area where it struck many pedestrians and finally came to a stop against several parked vehicles near the small observation tower over 200 feet southwest of the guardrail.

An examination of the dragster and its driver after the crash revealed the following. The uninjured 50 year-old driver had not been drinking and was in excellent condition before the race. He was an experienced driver and was very familiar with the mud drag strip and his dragster. This was his third year of competition at the motor sports park. At the time of the mishap, the driver was making his first competition run of the day. He advised that he did not realize that the car was in trouble until after the dragster's front end had risen into the air and after the car had rotated counterclockwise. Nothing felt unusual or erratic to him until that point. He immediately took his foot off the accelerator pedal but the dragster had already struck and mounted the guardrail. Due to the rapid speed and the poor visibility (normal) from the driver's low position in the cockpit, compounded by the flying mud and exhaust smoke, the driver could not see the sides of the drag strip to give him his bearings. Although the dragster was equipped with a driver-activated "kill switch,"

he did not have time to use the device. A kill switch is a device that will shut the engine down, thus stopping the vehicle.

The custom-built dragster underwent a pre-race inspection and a post-crash examination. According to the pre-race inspection conducted by its driver, an NMRO official and the driver's pit crew, the car met the 40-item check list. Specifically, the throttle, brakes, steering, and tires were all normal. The post-crash examination revealed that although the dragster had incurred multiple crash-induced damages, no defects among any components were detected which would have caused the car to go out-of-control.

As with any high performance motor sports competition, mud drag racing is not without risks to both the participants and the spectators. The objective of this type of racing is for the dragster, running alone at the time of the event, to be timed the fastest over the 200 foot long mud strip. Since this race is the top of its class, the dragster is more powerful and capable of higher speeds than other mud racing vehicles. The top speeds for these dragsters approach 130 mph and the time necessary to cover the strip is often below three seconds. Due to these tremendously powerful vehicles, fast speeds and short times, the margin for forgiveness and/or corrective action is severely limited. It certainly approaches and/or surpasses the human capabilities of drivers to react properly and safely when a problem such as lack of control does occur. As one insurance underwriter indicated, the frequency of serious accidents occurring with mud dragsters is low, but when they do occur they can be catastrophic as this crash at Bodacious illustrated. Fans of this type of racing find the sport thrilling due to the speeds involved and the potentially erratic

nature of the vehicle. The challenge is to ensure that the sport is as safe as possible for all concerned without sacrificing fan excitement and crowd appeal.

After the fatal crash, all drag races were discontinued on Saturday. On Sunday, when the races resumed, all vehicles and spectators were removed from the south side of the drag strip. Other remedial steps made by the promoter and a new sponsor sanctioning future events, the EMRA, included the placement of large "earth mover" tires along the finish line area on the north side of the track, and cutting the hillside sharply to create a stronger barrier between the dragsters and the spectators seated on the hillside. Spectators remain prohibited from the south side of the strip. Another major change at this particular motor sports park was shortening the length of the competition mud drag strip from 200 feet to 150 feet. Beginning in 1997, the length of the drag strip will be shortened to 125 feet as per EMRA sanctioning rules. The rationale for this change was to slow the speeds of the dragsters in hopes that if one became erratic, it would take less distance to stop. A major change at this park, as required by the new sanctioning body, is the use of remote controlled "kill switches" on these types of mud dragsters. The switches can be activated at the control tower whenever a dragster is seen erratically moving down the strip. Therefore, the likelihood of stopping the vehicle before reaching a barrier or spectators is greater.

According to those interviewed, the changes made at the Bodacious Motor Sports Park render it a near "model" facility in terms of spectator safety. This is partially due to the unique terrain of the park which allows the spectators to be elevated. Since the

landscape differs dramatically from facility to facility, any changes in the already established insurance and/or mud racing sanctioning bodies' guidelines which affect spectator safety would need to be evaluated at each park.

BODATIOUS MOTOR SPORTS PARK

NMRO Sanctioned and Insured

(Track at time of crash)

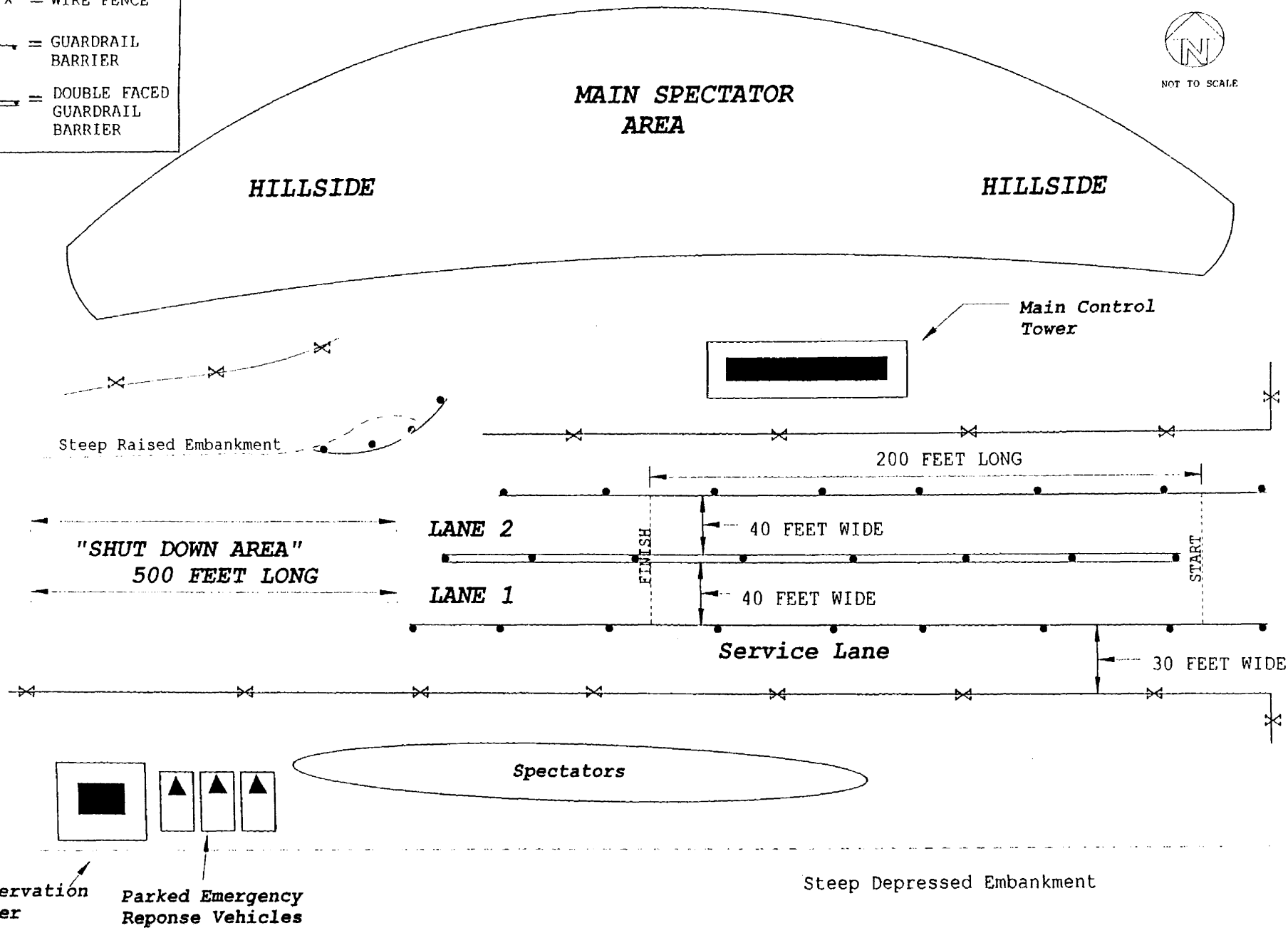
LEGEND

- *—* = WIRE FENCE
- = GUARDRAIL BARRIER
- == = DOUBLE FACED GUARDRAIL BARRIER



NOT TO SCALE

16



BODATIOUS MOTOR SPORTS PARK

(Crash)

LEGEND

- *—* = WIRE FENCE
- = GUARDRAIL BARRIER
- = DOUBLE FACED GUARDRAIL BARRIER



HILLSIDE

MAIN SPECTATOR AREA

HILLSIDE

Main Control Tower

Step Raised Embankment

LANE 2

FINISH

LANE 1

START

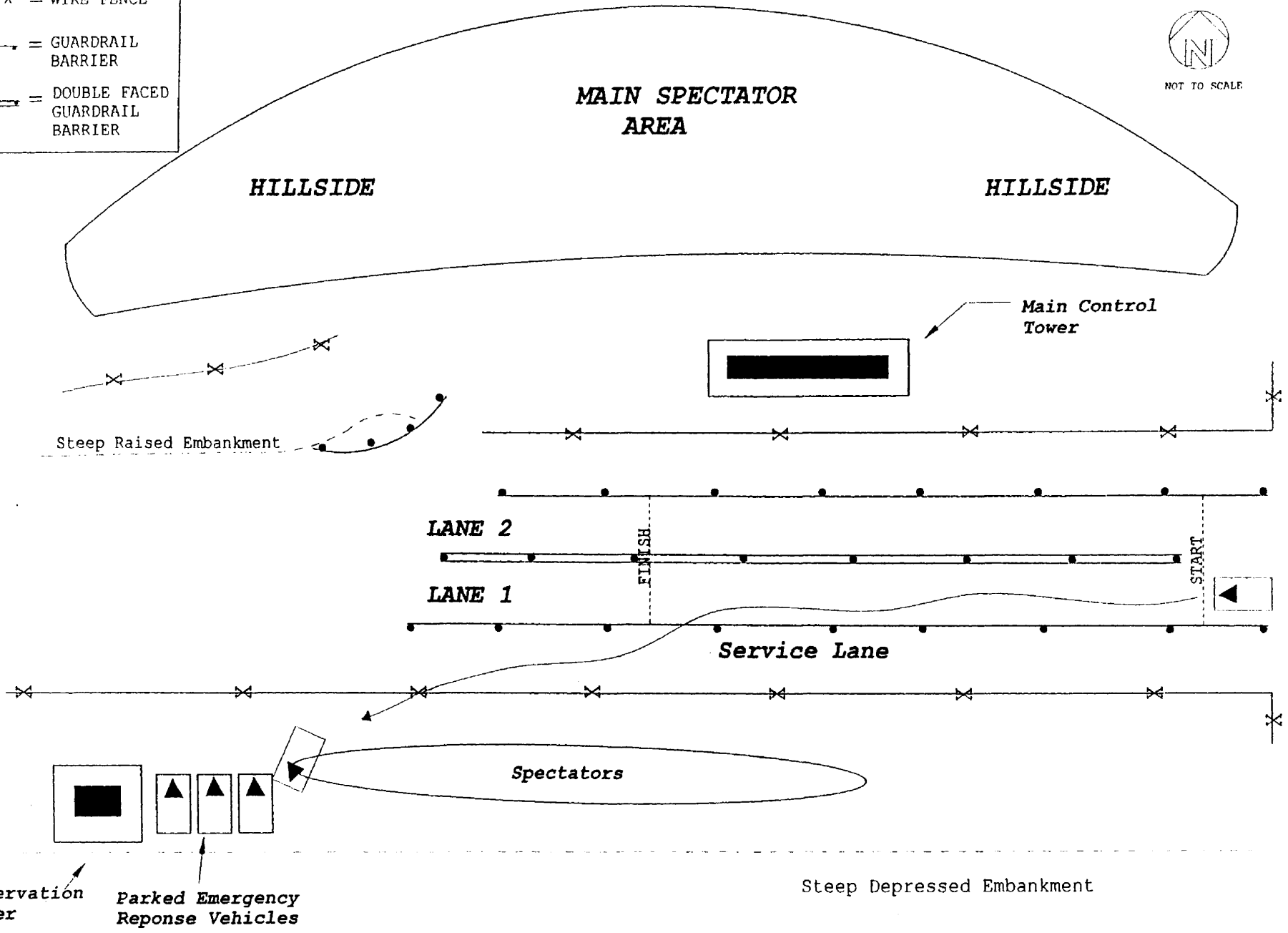
Service Lane

Spectators

Observation Tower

Parked Emergency Reponse Vehicles

Step Depressed Embankment



BODATIOUS MOTOR SPORTS PARK

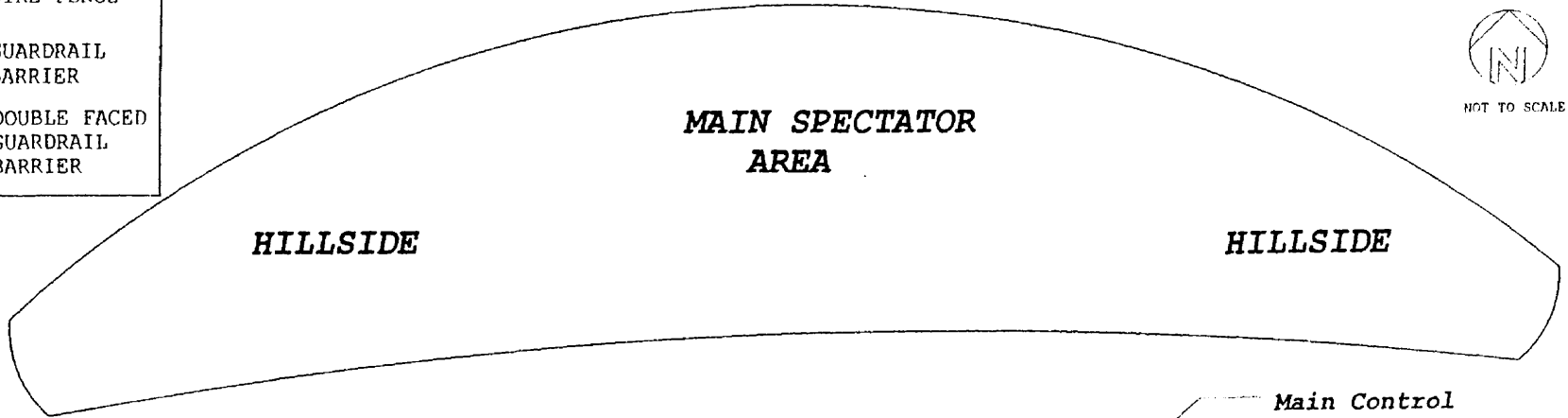
EMRO Sanctioned and Insured
 (Improvements made at track since crash)

LEGEND.

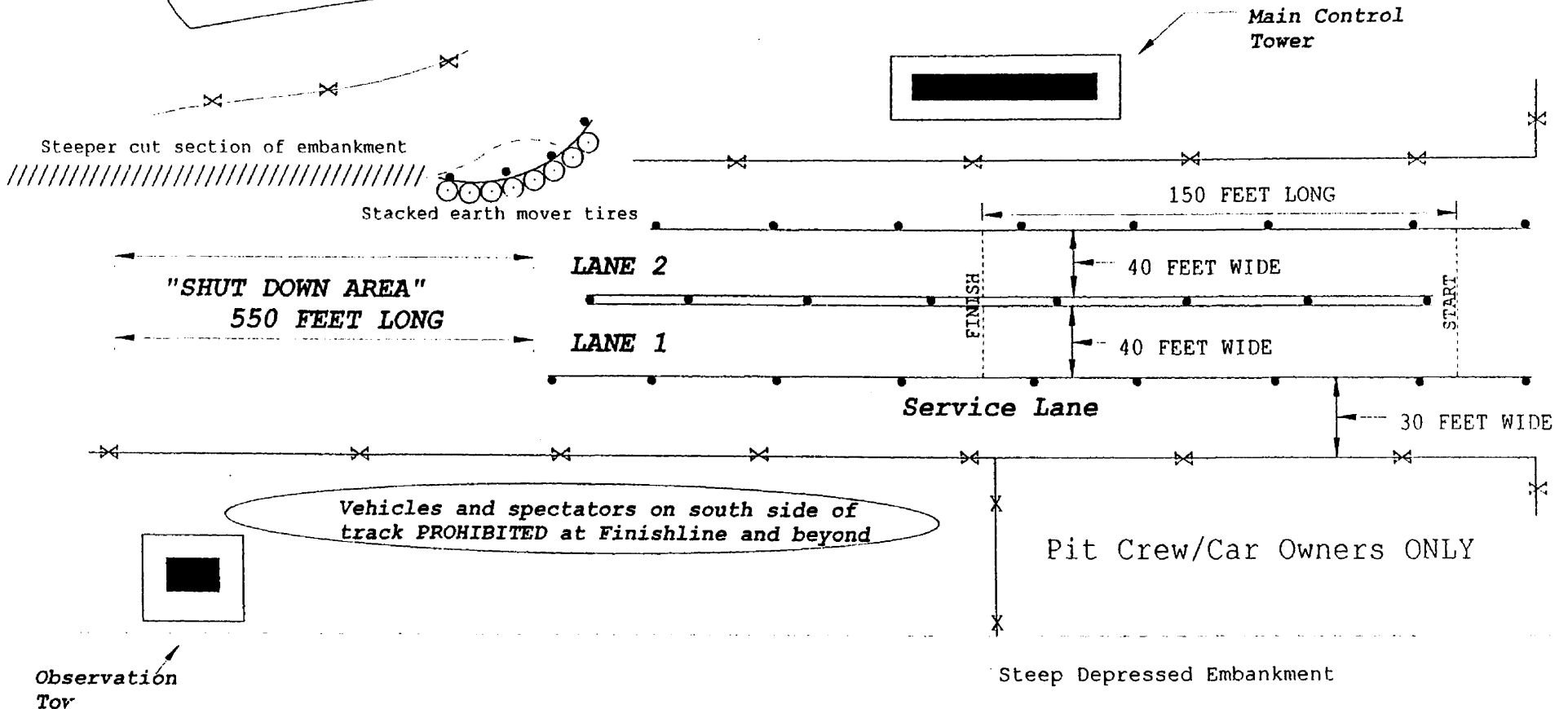
- *—* = WIRE FENCE
- = GUARDRAIL BARRIER
- = DOUBLE FACED GUARDRAIL BARRIER



NOT TO SCALE



18



APPENDIX

MUD BOGS/MUD DRAG/MUD SLING SPECTATOR SAFETY GUIDELINES

I. INSURANCE CARRIERS

1. Frazer Insurance Agency, Inc., Midlothian Virginia.

Referred to as Motorsports Racing. Insures mud racing events but only after the track promoter completes the application asking for detailed information including safety items. Photos and detailed sketch of race track and/or mud bog showing specific items such as pit areas and spectator areas must be included. They may send an inspector out to view the track and area.

2. K&K Insurance Group, Ft. Wayne Indiana.

Insures mud bog/mud drag racing. The promoter seeking insurance must complete a detailed insurance application diagramming the racing pit and spectator areas. A schematic showing a typical mud bog facility denoting minimum dimensions is submitted with the application. K&K sets forth minimum specifications that must be met to be insured by K&K. They also may send an inspector to ensure that the minimum guidelines are met. The following pertain to spectator safety:

A. Course Dimensions:

item #3: Positive barriers must be used separating dragsters from spectators.

item #5: A trap zone is required at the end of shutdown area to trap the dragster.

item #7: Crowd control fence must be installed separating race track and spectator areas. (Crowd control fencing can be chain link, woven wire, welded wire or of a nylon/plastic material. Supporting posts or poles may be a maximum of 10 feet apart and attached to fencing. Posts or poles are to be anchored in the ground.)

B. Spectator Area:

item #1: If event is held at an insured oval track, the event may be held so that the spectators remain in the same specifically designed spectator area that is normally used for track events, unless a bog area was specifically designed and accepted by K&K with the same or better than above protection standards.

C. Equipment Requirements:

Seven requirements specifically address the racing vehicle, tires, and safety equipment for the driver.

D. Competition Requirements:

Nine requirements specifically address the actual race. Four items address spectators.

item #4: Standard fire protection must be on-site for all events.

item #5: A transporting ambulance and two EMTs (or their equivalent) must be on-site for all events.

item #7: No persons are permitted at the end of and in line with the finish line and shut down area.

item #8: Pits must be self-contained and no spectators are permitted in the pits during competition. (Spectators may be permitted in the pits only for static display of vehicles and before or after competition is taking place.)

A closing paragraph on the K&K Insurance application form mentions that an inspector shall be permitted (but not obligated) to inspect the insured's property and operations for underwriting purposes at any time. Inspections are for the sole purpose of determining the insurability of certain property and operations and not safety. The insured is solely responsible for the safety of its facilities and operations and shall not rely upon any underwriting inspections to determine the safety of its track or operations and shall not diminish or forego its own safety practices and procedures.

3. George Knight and Associates, Inc., Tulsa, Oklahoma.

Insures mud-bog/drag races. The promoter seeking insurance must complete a detailed insurance application outlining the areas for spectators and pit facilities. These are minimum guidelines and if they are not met, insurance

coverage for the event will be denied. In addition to completing the application, a complete diagram with details of spectator fencing and all barriers with distances from track and spectator areas must be submitted. Additionally, photos of the mud bog, guardrails, spectator fencing and the start/finish lines shall accompany the application. GKA has specific mud bog/mud drag specifications.

A. Course Requirements:

item #4: Barriers must be erected on each side of bog separating the dragsters from the spectators, from start to finish. Barriers must be concrete or double strand highway steel or DOT barrier. If concrete, sections must be pinned together with satisfactory reinforcing rods.

item #8: Pit area may be to either side of competition area, behind spectator area or behind the start line. If pits are on one side and spectators on the other, the same fencing is required on both sides.

B. Equipment Requirements:

Four requirements mention the dragster and require the driver to be belted.

C. General Information:

item #1: Absolutely no person will be permitted beyond the finish line during competition.

item #3: Any person entering the pit area must be required to sign a valid release. Spectators are not allowed in the pit area during competition.

D. Spectator Area:

item #1: Spectator areas can be on either or both sides of bog.

item #2: Crowd control fence must be a minimum of 30 feet from the pit barrier and parallel to same.

item #3: Acceptable crowd control fence may be chain link, woven wire or welded wire. Supporting poles may be up to 10 feet apart but must be anchored in the ground.

item #4: If event is held in a rodeo arena or a race track currently insured, it is permissible to allow spectators to sit in the stands where they would normally sit. However, no person will be allowed to sit beyond the finish line of the competition zone.

A schematic showing a typical mud-bog facility denoting required minimum dimensions is submitted with the application.

4. Allied Specialty Insurer, Inc., Treasure Island, Florida.

Insures mud bog/mud drag races. The track or event promoter will be sent a detailed motor sports application outlining the area for spectators and pit facilities.

These minimum guidelines -- based on historical claims, experience in these areas and sound "common sense" -- must be met to purchase insurance coverage. They may or may not send a representative to ensure that the track meets their qualifications. A map and photos are also required outlining the separation between

the participants and the spectators. These are submitted with the application. These guidelines are as follows:

Barrier: All race facilities will be protected by barriers between the race vehicle and the viewing area. Barriers will be constructed of concrete (3 feet high and 8 inches wide at the top), guardrail (3 feet high), or wood (3 feet high and 4 inches wide at the top).

Fencing: All race facilities will have a spectator fence 6 feet in height, no less than 40 feet away from the barrier opposite the racing surface. Fencing for crowd control shall be a minimum of six feet in height and protect spectators from entering any restricted areas.

Shut Down: The end of the shut down area will have no exit to spectator property. No one is allowed to sit or stand within 40 feet of the shut down area. This area must also be fenced off. No one is allowed to park vehicles at the end of the shut down area. No racing can take place while the shut down area is occupied.

II. SANCTIONING BODIES

1. National Mud Racing Organization, Inc., Urbana, Ohio.

(1993 Official Rule Book -- The sanctioning body for the 1995 Bodacious mud racing event on the date of the fatal crash).

Both participant and spectator safety in general were noted on the rule book's first two pages. The only specific minimum guidelines mentioned involved

the shut down area which states “The shut down area shall be clear of people and machines during each run. No spectators at end of shut down. No crossing permitted at end of race pit.”

2. The Mid-Atlantic four-wheel Drive Association 1992 Association Rules (and 1986 Handbook).

Although this organization had disbanded at the time of the fatal Bodatious event, its safety guidelines were used for the Bodatious race track and spectator areas. Relevant portions are as follows: “Rules for Track and Spectator Safety to be used at all times:”

Mud sling/Mud bog:

item #2: Starting Line:

- a. Spectators are to be kept in the area of a 75 foot radius from the center of the starting line, or
- b. Right angle must be formed at a point 25 feet parallel with the strip and 25 feet behind. No spectators are allowed in this area.
- c. Four foot wall above strip level with a permanent barrier on spectator side, or
- d. Physical barrier capable of stopping vehicle within 10 feet of spectators.
 1. Guardrail
 2. Tires stacked three feet high filled with dirt

3. Any option as long as it still stops vehicle and stands up to spectator abuse.

e. Crosswalk to be 100 feet behind starting line.

item #3: Return lane:

- a. Unless lane goes completely around and behind the spectators, it must be within sling (mud drag) strip and starting line restrictions.
- b. Speed limit to be held at 7 ½ miles an hour.

Note: If spectators are on both sides of the track, rules must apply to both sides with designated crosswalks at least 100 feet behind starting line with track marshals to supervise crossing of staging lanes.

3. Eastern Mud Racing Association, Cream Ridge, New Jersey.

The EMRA sanctions several mud racing events in Virginia. A rule book is available for drivers that outlines among other items, the mud dragster requirements and safety concerns for the driver. A schematic outlining the placement of spectators and the mud pit dimensions is also available denoting the following:

- a. Mud pit dimensions: 125 feet long and 25 feet wide with a depth of eight inches at the pit's entrance (starting line) and one foot at the exit (finish line). At the entrance end, the pit shall be constructed on a downgrade slope

at least 16 feet long and at the exit end on an upgrade slope at least 30 feet long. The maximum depth of the sunken pit will be three feet.

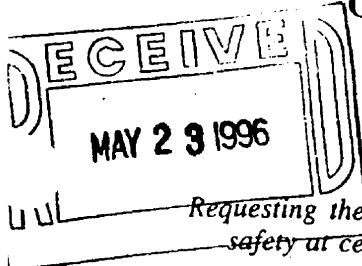
- b. The shut down area beyond the finish line will have a minimum distance of two times the pit length.
- c. The entrance to the pit area off the shut down area will be to the drivers left and built on a 90 degree turn.
- d. The spectator areas, permissible on either side of the mud drag strip, shall be a minimum of 50 feet from the pit and extend from before the starting line to 25 feet before the finish line. The spectator areas (bleachers and/or positions on the ground) shall be separated from the pit by two 36-inch high concrete Jersey barrier walls and/or two 32-inch high metal guardrail systems. One barrier will parallel the extreme edge of the pit from start to 30 feet beyond the finish line, and the second barrier shall be 30 feet from the first barrier and will start before the starting line and end beyond the finish line at the shut down area. A chain link fence four feet high (or its equivalent) shall separate the spectator area from the mud drag pit. Both the fencing and bleachers (if used) are to be tapered away from the pit at a near 45 degree angle starting at the 100-foot mark along the pit. This is to further protect the spectators near the finish line where the dragster's speeds are the fastest and where the highest risks associated with loss of control are located.

4. New Jersey Department of Law and Public Safety, Division of State Police - Racing Control Unit, Trenton, N.J.

The State of New Jersey, by statute, requires the Division of State Police to regulate and enforce certain state mandates on motor vehicle racing. As it specifically relates to mud drag racing (referred to in New Jersey as “mudhop” events) the following regulations for spectator safety were derived:

- a. Guardrail barriers (referred to as “hubrails”) separating the mud dragsters from the spectator areas are constructed with specific dimensions, such as being at least two feet high.
- b. All fences to separate spectator areas from the racing area shall be constructed of welded wire fabric or chain link and shall be six feet in height and constructed so as not to be easily lifted, climbed over or moved aside.
- c. All spectator areas must be at least 25 feet from the guardrail barrier.
- d. All guardrail, fences, stands and buildings shall be constructed and maintained so as to afford maximum protection for spectators.

In addition, all mud drag racing events shall only be conducted at licensed facilities having liability insurance coverage. Numerous other regulations concerning the racing vehicle, driver, and facility are stated in their code sections.



HOUSE JOINT RESOLUTION NO. 87

Requesting the Virginia Commonwealth University Transportation Training Center to study spectator safety at certain racing competitions.

Agreed to by the House of Delegates, March 7, 1996

Agreed to by the Senate, March 6, 1996

WHEREAS, on May 6, 1995, three people were killed and six others injured when an off-the-road racing vehicle, participating in a race at the Bodacious Motor Sports Park in Cumberland County, vaulted over the guard rail; and

WHEREAS, the physical condition of the race track, the construction of and modifications to the participating vehicles, the proximity of the spectators to the race track, and the construction and location of barriers separating the spectators from the race track all combined to produce tragic results; and

WHEREAS, it is highly desirable that appropriate steps be promptly taken to ensure the safety of spectators at similar events in the future; and

WHEREAS, while no single agency of state government is charged with the unified and comprehensive responsibility for the safety and protection of spectators at racing competitions or similar spectator sporting events, the Virginia Commonwealth University Transportation Training Center has expertise that may productively be brought to bear on spectator safety at racing competitions; now, therefore, be it

RESOLVED by the House of Delegates, the Senate concurring, That the Virginia Commonwealth University Transportation Training Center be requested to study spectator safety at racing competitions. The Center shall focus particularly on events involving "monster cars" and "mud dragsters," and shall consider whether some form of regulation of these events and the facilities where these events are held is in the interest of reducing the risk of injury to spectators. However, speedways and racetracks where races sanctioned by NASCAR and the National Hotrod Association are held shall be excluded from this study. The Board of Housing and Community Development shall provide technical assistance to the study, upon request. Other agencies which have experience or expertise relevant to this study shall provide assistance, upon request. In conducting the study, the Center shall seek the participation of representatives of the Virginia Municipal League and the Virginia Association of Counties.

The Center shall complete its work in time to submit its findings and recommendations to the Governor and the 1997 Session of the General Assembly as provided in the procedures of the Division of Legislative Automated Systems for the processing of legislative documents.



Virginia Commonwealth University

July 16, 1996

CENTER FOR PUBLIC POLICY

TRANSPORTATION SAFETY
TRAINING CENTER
921 WEST FRANKLIN STREET
P.O. BOX 843023
RICHMOND, VIRGINIA 23284-3023

804 828-6235
FAX: 804 828-1848
TDD: 1-800 828-1120

Delegate David G. Brickley
4310 Ridgewood Center Drive
Woodbridge, VA 22192

Dear Delegate Brickley:

The Transportation Safety Training Center (TSTC) has recently been in contact with your aide concerning our involvement in House Joint Resolution Number 87 - the study of spectator safety at certain "monster car" and "mud dragster" racing events. She provided us with some very useful information, in addition to sending us clippings on the tragic Bodatious incident that was the catalyst behind this resolution.

We would like to extend our appreciation to you on the assistance and direction shared with us by your staff.

This letter will serve as an implementation plan concerning the center's involvement in this matter. We will contact and hopefully meet with representatives from the Virginia Department of Housing and Community Development, the Virginia Municipal League and the Virginia Association of Counties. Attempts will be made also to contact related off-road racing groups and/or enthusiasts. We hope to identify the number of locations in Virginia where similar complexes such as Bodatious exist and determine what, if any, spectator safety innovations are being implemented. We will also attempt to determine and contact relevant national organizations governing or sanctioning these types of mud-bog racing events in an effort to see **what** implementations are being used for spectator protection. The Center will also focus on the basic circumstances surrounding the Bodatious mishap in an attempt to develop a case study for comparison purposes.

After all of these items can be gathered and reviewed, it will be our intent to answer the questions posed in the resolution and as shared via your aide. These are: 1) what present Virginia regulations (if any) are in effect that would ensure the safety of spectators at these events, 2) are there any National standards or guidelines used by similar racing groups or states for these purposes, and 3) should

Page Two
Delegate David G. Brickley
July 16, 1996

Virginia consider whether some form of regulation of these events and facilities is warranted in the interest of reducing the risk of injury to spectators.

The Center shall complete its study and submit its findings and recommendations to the Governor and the General Assembly before the 1997 session convenes.

If you have any questions or would like to discuss further, please advise.

Sincerely,



Robert J. Breitenbach
Assistant Professor/Director
Transportation Safety Training Center
Virginia Commonwealth University



David O. McAllister
Leader/Instructor
Virginia Crash Investigation Team
Virginia Commonwealth University
(804)828-6235

DOM/dlc

cc: Donald C. Gearing

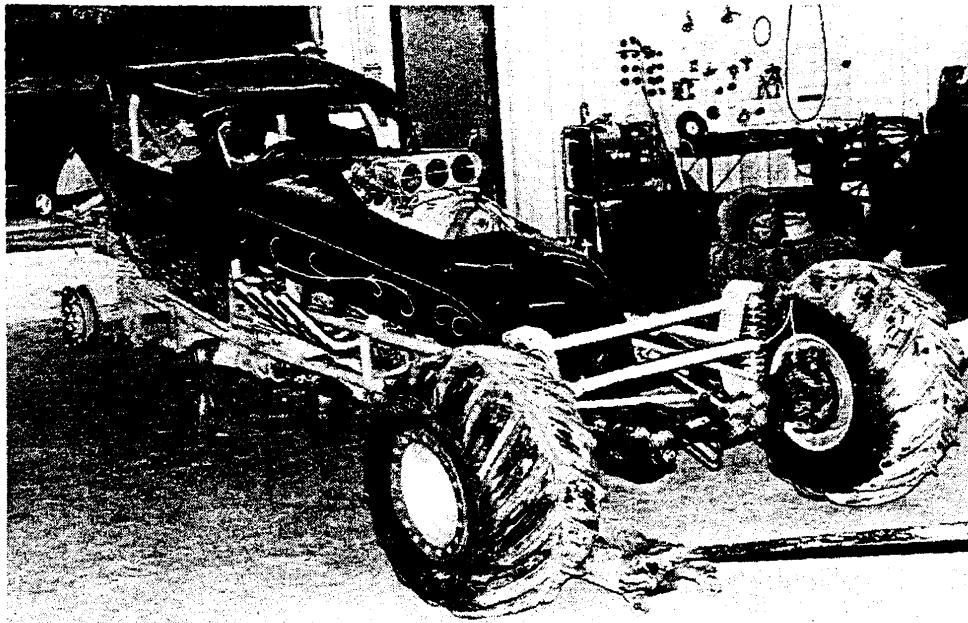


Photo #1: Side view of the mud dragster involved in the fatal Bodatious crash. (Photo taken after the crash.)

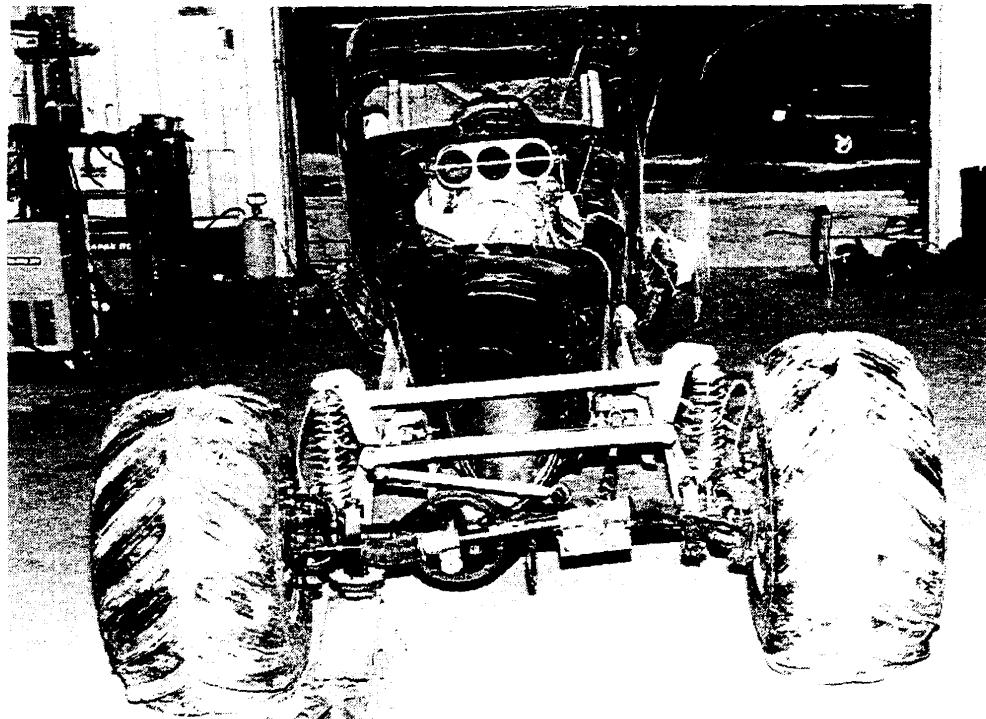


Photo #2: Front view of mud dragster involved in fatal Bodatious crash.



Photo #3: Bodacious Motor Sports Park, taken one week after the fatal crash. View looking west from the starting line of lane number 1. Spectator casualties were located in left of photo, now a prohibited area. Spectators were permitted on hillside in right of photo.

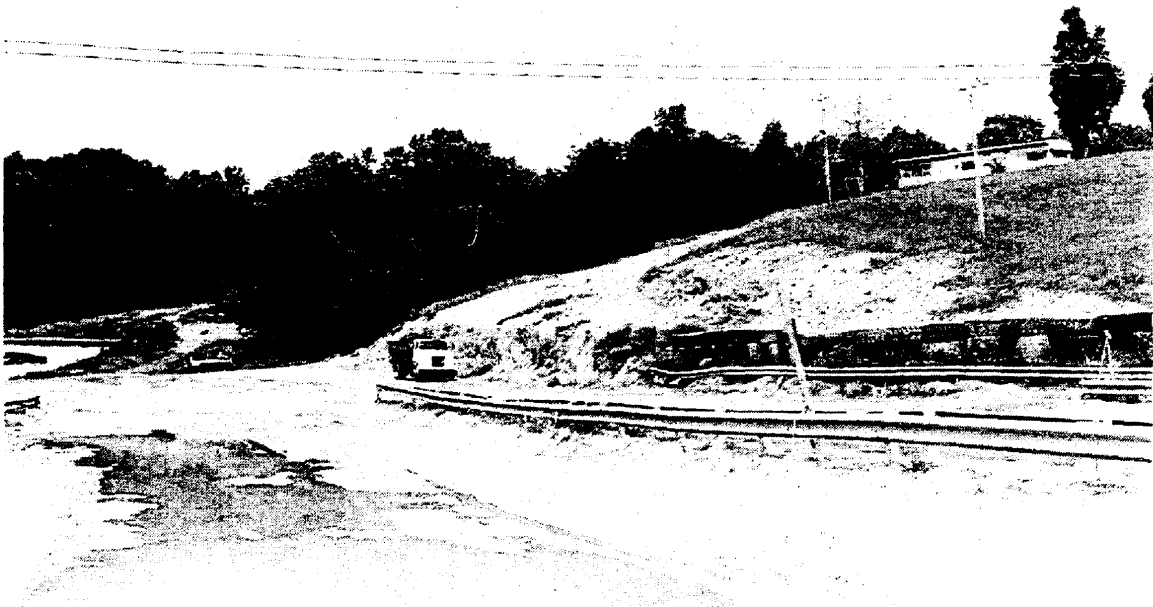


Photo #4: View of Bodacious Park taken in Summer, 1996 looking west from near the finish line. Note embankment, hillside and large tires in right of photo. Spectators were permitted on hillside.



Photo #5: View looking from starting line to finish line at Charles City 4X4 mud drag strip. Note depth of single mud bog and barriers.



Photo #6: View looking from vicinity of start line showing spectator area at Charles City mud drag strip. Spectators, separated from the strip by an embankment, guardrail barrier and chain link fence, are about 50 feet from the mud bog.



Photo #7: View of mud bog in near Stony Creek, Virginia showing single mud strip with guardrail (in background) and spectator area (in foreground).



Photo #8: Closer view of #7 showing mud strip, guardrail and fencing separating spectator area. The spectators are located over 100 feet from the mud drag strip.



THE MID-ATLANTIC
4-WHEEL DRIVE ASSOCIATION

1992


ASSOCIATION RULES

RULES FOR TRACK AND SPECTATOR SAFETY TO BE USED AT ALL TIMES:

MOTORCROSS

1. Starting Line
 - a. 25 foot radius recommended without physical barriers present.
 - b. Physical barriers must be 10 feet from the track
 - c. Spectator barriers are to be 20 feet from physical barriers.
2. Curves:
 - a. Physical barrier and safe distance beyond.
3. Tracks:
 - a. Any side by side motorcross track will be required to measure at least 40 feet width on 50% of the track and the remaining must be at least 20 feet in width.

NOTE: Any place on the track that might be reasonably expected for a vehicle to leave the track and enter the spectator area, that place must have a natural or man-made barrier or NO SPECTATORS ALLOWED.

 MUD SLING

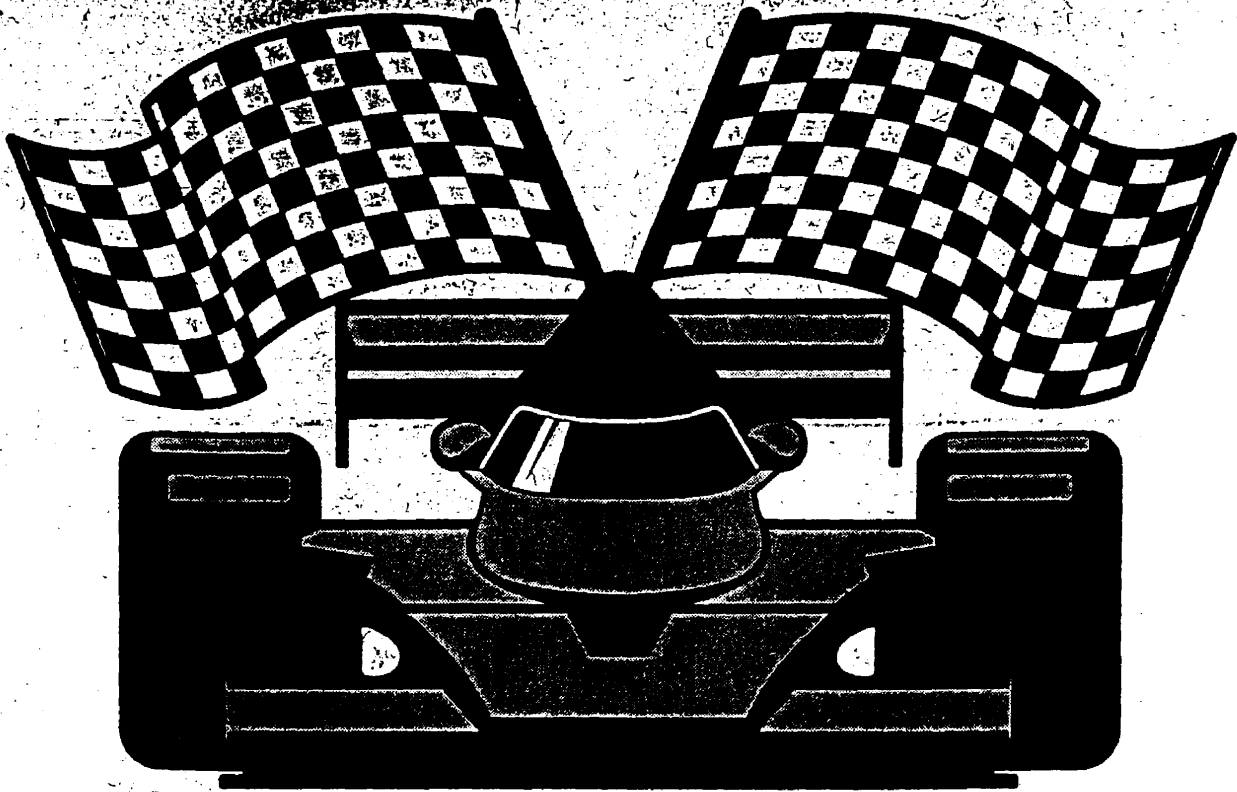
1. All slings will be 200 feet in length with no less than 300 feet of run out area. The running surface will be no less than 40 feet in width. Return roads in good condition must be provided.
2. Starting Line:
 - a. Spectators are to be kept in the area of a 75 foot radius from the center of the starting line or
 - b. Right angle must be formed at a point 25 feet parallel with the strip and 25 feet behind. No spectators are allowed in this area.
 - c. Four foot wall above strip level with a permanent barrier on spectator side or
 - d. Physical barrier capable of stopping vehicle within 10 feet of spectators.
 - 1) Guard Rail
 - 2) Tires stacked three (3) feet high filled with dirt
 - 3) Any option as long as it will stop vehicles and stand up to spectator abuse.
 - e. Crosswalk to be 100 feet behind starting line.
3. Return Lane:
 - a. Unless lane goes completely around behind spectators it must be within sling strip and starting line restrictions.
 - b. Speed limit to be held at 7½ miles and hour.

NOTE: If spectators are on both sides of the track, rules must apply to both sides with designated crosswalks at least 100 feet behind starting line with track marshalls to supervise crossing of staging lanes.

PRIOR TO THE EVENT:

1. Two or more MA officials will inspect the track and deem event will be within the MAA safety standards. If not within safety standards, it must be corrected by race time or event cannot be held as an MAA sanctioned event. All tracks must be sanctioned thirty days prior to their event.
2. Staging area must be separate from the spectator area.

CHAPTER 62



MOTOR VEHICLE RACE TRACK REGULATIONS

August, 1996



Christine Todd Whitman
Governor

Peter Verniero
Attorney General
38

Colonel Carl A. Williams
Superintendent

NEW Jersey Admin. Code

**MOTOR VEHICLE RACE TRACK REGULATIONS
N.J.A.C. 13:62.1 et seq.**

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SUBCHAPTER 2. MOTOR VEHICLE RACE TRACK LICENSE REQUIREMENTS & LICENSEE RESPONSIBILITIES (ALL RACING EVENTS)

13:62-2.1 License application procedure

- (a) A license shall be required for any operation or conduct of motor vehicle races and exhibitions of motor vehicle driving and the tracks or places at which the same are operated and conducted. The application for a motor vehicle race track license must be submitted at least 90 days prior to the first day of racing or exhibition. An application for renewal of a license shall be submitted within 60 days of the expiration date of the license and is to be accompanied by:
1. An insurance certificate;
 - i. N.J.S.A. 5:7-13 states that no license shall be issued for the holding of any motor vehicle race or exhibition of motor vehicle driving skill until the person applying for the license shall have filed with the department evidence satisfactory to it of the issuance of an insurance policy approved by the department and conditioned, in the case of a race or exhibition at a track or other place accommodating less than one thousand people, for the payment of less than \$25,000.00 to any one person obtaining judgment, and not less than \$50,000.00 on all judgments recovered, and for the payment of not less than \$50,000.00 to any one person obtaining judgment, and not less than \$100,000.00 on all judgments recovered, in the case of all other races or exhibitions, upon a claim or claims arising out of the same transaction, connected with the same subject of action (to be apportioned ratably among judgment creditors according to the amount of their respective judgments) for damages because of bodily injury, including death at any time resulting therefrom, caused to any person or persons, other than the driver, and all pit area personnel as the result of an accident occurring as a result of the conduct of any motor vehicle race or exhibition of motor vehicle driving skill, for which the license is granted, under which policy the liability of the company shall become absolute when loss or damage covered by the policy occurs, and satisfaction by the insured of a final judgment for the loss or damage shall not be a condition precedent to the right or duty of said company to make payment on account of the loss or damage and which policy shall not be cancelable or annulled as to any loss or damage by an agreement between the carrier and the insured after the insured has become responsible for the loss or damage or in any other event, except on ten days prior notice to the department.
 2. A duplicate of the insurance policy;
 - i. The policy shall be issued by a company approved by the Superintendent;
 - ii. The policy and the certificate shall contain a statement to the effect that they are noncancelable except upon 30 days prior written notice to the Superintendent;
 3. A certified check or postal money order in the amount prescribed by law as the license fee;
 4. A certification from the building inspector of the municipality where the track is located to the effect that he has inspected the spectator seats and found them safe for use. Where the municipality does not have a building inspector, or a building code, a certification from a New Jersey licensed structural engineer may be accepted; and
 5. A sketch or sketches of the track and associated areas, as near to scale as practicable, indicating the location of required safety features such as hub rails, fences, light or flagmen positions, spectator seating, entrances and exits, pit facility locations and other physical factors affecting the safety of spectators and participants. This requirement shall not apply to locations licensed prior to January 1, 1963, unless alterations have been made to the track and associated areas on or after January 1, 1963.

13:62-2.2 Licensee's responsibility

The licensee is responsible for any violations of N.J.S.A. 5:7-8 et seq. or any of the provisions of this chapter.

13:62-2.3 Restrictions upon license

- (a) The Superintendent may impose reasonable restrictions upon any licensee.
- (b) The restrictions may include, but shall not be limited to:
 1. Requirements for special protective devices for the participants in, or spectators attending, any race or exhibition; and/or

2. Limitations concerning spectator area; and/or
 3. Limitations concerning types of events and classes of vehicles; and/or
 4. Requirements for the protection of participants and spectators.
- (c) The licensee will comply with any special restrictions imposed by the Superintendent after receiving written notice thereof from the Superintendent.

13:62-2.4 Procedures for approval of unspecified events

- (a) When a licensee requests approval to conduct a racing event as defined herein which is not specifically addressed by these rules, the licensee shall apply to the Superintendent for approval for the conduct of said event. Approval shall not be granted unless the Superintendent is satisfied that:
1. The request for an approval was made in writing to the Superintendent at least 20 days prior to the racing event;
 2. The request contains a sufficient description of the event; and
 3. The approval will not adversely affect the safety of the public or participants at a racing event.

13:62-2.5 Infield pit area; inspection

- (a) Tracks having the pit area in the infield of a substantially circular or oval track will be subject to special inspection to determine whether arrangements are sufficient to provide reasonable protection for pit area personnel.
- (b) Such inspection will be made following application to the Superintendent which shall be filed in conjunction with the application for the race track license.

13:62-2.6 Alcoholic beverages and drugs

- (a) No alcoholic beverages, narcotic, hallucinogenic or habit producing drug will be permitted on the race track proper, the pit area, the staging lanes or any other area having unrestricted access to the race track proper. No person who has partaken of any alcoholic beverage or narcotic, hallucinogenic or habit producing drug in any amount whatsoever shall participate in any race or exhibition of driving skill, or perform any duties in the pit, pit area, or staging lanes.
- (b) Any vehicle containing alcoholic beverages or narcotic, hallucinogenic or habit producing drug, and any person found to have partaken of alcoholic beverages or narcotic, hallucinogenic or habit producing drug shall be removed from the pit area as soon as is practicable and shall be prohibited from returning to the pit area.
- (c) No person who has been convicted of the use or possession of a controlled dangerous substance, as provided in the Comprehensive Drug Reform Act of 1986, N.J.S.A. 2C:35-1 et seq., shall be permitted to enter into a pit area for a period of one year from the date of conviction.

13:62-2.7 Pit credentials

- (a) The track management shall be responsible for the issuance of pit credentials and only those credentials issued by the track management shall be recognized as valid.
- (b) Only persons holding pit credentials shall be admitted to the pit area or racing area.
- (c) Reporters and photographers of the press in possession of a valid pit credentials shall be limited to safe areas.
- (d) The issuance of pit credentials shall be limited to:
1. Mechanics assigned to race cars with a maximum of four to each car;
 2. Drivers having cars entered in one of the events in the race;
 3. Members of the track staff having business in the pit area;
 4. Race officials; and
 5. Accredited photographers and reporters of the press.
- (e) The track management shall be responsible for checking credentials of persons entering the pit area to ensure that no person enters the pit area or engage in a race or exhibition unless such person produces credentials showing age which satisfies the age requirement for a particular event.
- (f) The licensee or the Superintendent may require a person to produce satisfactory evidence attesting to said person's physical and mental well-being as prerequisite to obtaining permission to enter the pit area. Such evidence shall be on a form signed by a New Jersey licensed physician.
- (g) No one under the age of 18 is to be permitted in the pit area, staging lanes or track surface.
- (h) Flagmen and starters at all racing events shall be at least 18 years of age.
- (i) Mechanics must wear pants while working on their respective vehicles.

- (j) Drivers are not permitted to wear shorts while competing.
- (k) Track staff will wear pants, at all times, while working in a race event.

13:62-2.8 Announcements

- (a) The licensee shall make a suitable announcement over the public address system in the pit area, advising the public and pit personnel of the following:
 - 1. The minimum age of persons permitted in the pit area;
 - 2. The prohibition against smoking in the area where fuel is stored or refueling takes place;
 - 3. The prohibition against the use of alcoholic beverages in the pit area; and
 - 4. The prohibition against the use of narcotic, hallucinogenic or habit producing drugs in the pit area.
- (b) The announcement is to be made approximately 15 minutes before the start of the day's program and twice during the program.
- (c) The licensee shall post a sign at all entrances to the pit area advising authorized persons in the pit area of the following:
 - 1. The minimum age of persons permitted in the pit area;
 - 2. The prohibition against the possession or use of alcoholic beverages or narcotic, hallucinogenic or habit producing drugs in the pit area; and
 - 3. The prohibition against smoking in areas where fuel is stored or refueling of vehicles takes place.

13:62-2.9 Monthly reports

The licensee shall file with the Superintendent a monthly report on the form approved and provided by the Superintendent. This report shall include the date of racing events held in the reported month, the type of event, the attendance, the number of vehicles as well as an account of any other unusual incidents occurring at the track during the reporting period. The report shall further include the monthly schedule of events.

13:62-2.10 Accident reports and impounding of certain vehicles

- (a) The licensee shall report accidents involving injury or death must be reported to the office of the Superintendent, by telephone, no later than the first business day following the accident.
- (b) Such a report must be followed within 48 hours by a complete written report of the accident.
- (c) Any vehicle which is involved in a crash resulting in serious or fatal injuries to a driver or spectators thereof shall be impounded by the licensee and detained until such time as an inspection of the vehicle may be made by a representative of the Superintendent.

13:62-2.11 Report of deaths to local police

In addition to the reports to the Superintendent, the licensee shall report any accident resulting in a fatality to the police agency having jurisdiction by the quickest means available.

13:62-2.12 Inspection of vehicles

- (a) The licensee shall arrange for the inspection of each participating vehicle prior to the event, to determine that it meets the requirements of this chapter. Vehicles not meeting the requirements set forth for the specified event shall be barred by the licensee from participating or practice.
- (b) Vehicles which are to be used in automobile races or exhibitions of driving skill are subject to unannounced inspection and approval at any time by the Superintendent or designee.

13:62-2.13 Braking system and pedal reserve

- (a) The licensee shall test and approve each race car for brake pedal reserve before the car leaves the pit area to enter the track.
- (b) The licensee shall not permit any vehicle to participate in any race or exhibition if the braking system includes the direct application of pressure to any of the tires or with any apparent deficiency.

13:62-2.14 Refueling

- (a) In all instances where refueling is permitted with the engine running, the licensee shall insure that a member of the pit crew equipped with an approved type fire extinguisher to be in close proximity to the fill pipe of the fuel tank.
- (b) The licensee shall prohibit smoking in any area where fuel is being transferred or stored.
- (c) The licensee shall prohibit the use of welding and acetylene torches in any area where fuel is being transferred or stored unless a fully charged fire extinguisher is in close proximity.

13:62-2.15 Water overflow tanks

The licensee shall prohibit water overflow tanks or reservoirs to be installed inside the driver compartment. Tanks or reservoirs mounted in the roll cage must be fully shielded to protect the driver.

13:62-2.16 Security personnel

- (a) The licensee shall furnish sufficient security protection to maintain peace and good order.
- (b) The licensee shall furnish guard personnel at each unlocked gateway between the spectator areas and the track and/or pit areas.
- (c) The licensee shall furnish guard personnel at each unlocked gateway between the pit area and the track area.

13:62-2.17 Ambulances; first aid attendant

- (a) The licensee shall not permit any race or exhibition of driving skill unless there is available for immediate use at the licensed location at least one vehicle suitable for ambulance purposes, together with two trained first aid attendants.
- (b) The licensee shall ensure that a driver of any racing event involved in an accident which results in considerable damage to the vehicle or any noticeable injury to the driver is checked by the first aid attendant(s) on duty and cleared to participate before he/she returns to the racing event.
 1. If the driver refuses medical attention, he or she must sign a waiver stating so.
 2. The licensee shall not allow the driver to re-enter the race event if the driver refuses to sign the medical waiver or medical attention is recommended.

13:62-2.18 Fire fighting equipment

- (a) The licensee shall prohibit any race or exhibition of driving skill to be conducted unless there is available at suitable locations around the track Class B Underwriter labeled approved fire extinguishers.
- (b) All fire extinguishers shall be fully charged at the beginning of each day's activities.
- (c) The licensee shall inspect all fire extinguishers at least once a year and affix a label to show the date of inspection
- (d) In addition there shall be a reserve consisting of a recognized paid or voluntary fire company with their equipment or at least 350 pounds of dry chemical available to move to the scene of any major fire.

12:62-2.19 Wreckers

- (a) Only authorized personnel may ride on any wrecker.
- (b) No person shall ride outside the cab of any wrecker.
- (c) Wreckers shall be operated with due care and circumstances.

SUBCHAPTER 3. RESERVED

SUBCHAPTER 4. SAFETY REQUIREMENTS FOR VEHICLES & PERSONNEL: OVAL RACING

13:62-4.1 Construction requirements

- (a) Hubrail construction shall comply in all respects with the requirements of this chapter.
- (b) The licensee shall provide and maintain hubrails on the outer circumference of the track and around the entire circumference thereof. Where spectators are allowed in the infield or within the inner circumference of the track, the licensee shall provide a hubrail, as described in this section, around the inner circumference of the track.
- (c) The hubrail shall consist of at least two planks of hardwood or other suitable materials, at least 10 inches in width by three inches in thickness.
- (d) The hubrail shall be supported by posts of similar material of at least six inches in width and six inches in thickness or round posts not less than seven inches in diameter, which are set in the ground at least four feet and shall extend above the ground at least two feet. The post shall be no higher than the hubrail planking and shall be spaced no more than six feet apart.

SUBCHAPTER 11. MUD HOP *

13:62-11.1 Construction requirements

- * (a) Hubrail construction shall comply in all respects with the requirements of this chapter.
- (b) The licensee shall provide and maintain hubrails on the outer circumference of the track and around the entire circumference thereof. Where spectators are allowed in the infield or within the inner circumference of the track, the licensee shall provide and maintain a hubrail, as described in this section, around the inner circumference of the track.
- (c) The hubrail shall consist of at least two planks of hard wood or other suitable materials, at least 10 inches in width by three inches in thickness.
- (d) The hubrail shall be supported by posts of similar material of at least six inches in width and six inches in thickness or round posts not less than seven inches in diameter, which are set in the ground at least four feet and shall extend above the ground at least two feet. The post shall be no higher than the hubrail planking and shall be spaced no more than six feet apart.
- (e) Two planks of hardwood or other suitable material shall be mounted on the side of the post facing the track and running horizontally and parallel to each other.
- (f) On the opposite side of the posts, not more than eight inches from the top thereof, there shall be a 3/4 inch steel cable running around the circumference of the track and securely fastened to the post with eye bolts.
- (g) The hubrail entrance and exit gates to the pit area shall be closed while vehicles are in motion on the track, unless alternate arrangements have been made by the installation of barriers of a type which will prevent cars out of control from leaving the track and entering the immediate pit working area.
- (h) Where the licensee wishes to use methods other than gates, an inspection of such installations by a representative of the Superintendent is required.
- (i) The hubrail opening for vehicles shall be so located that a vehicle leaving the track must turn 90 degrees before entering the pit area.
- (j) At locations using methods other than gates between the pit area and the track, the licensee shall provide a guard to prevent unauthorized persons from entering the track area.
- (k) The use of baled hay or straw or any similar material as a protective device between participating vehicles and spectators is specifically prohibited.

13:62-11.2 Fences

- * All fences installed for the purpose of limiting spectator areas shall be constructed of welded wire fabric or chain link and shall be at least six feet in height and so constructed as not to be easily lifted, climbed over or moved aside, except at motocross events the fence shall be of the same construction but at least five feet in height.

13:62-11.3 Red and amber lights

- (a) Each track used for automotive racing, except those used for acceleration and performance tests, must be equipped with a system of at least four red lights and four amber lights so arranged that at least one light of each color will be visible to the drivers as they enter each turn.
- (b) The lights shall be controlled by a single switch. The licensee shall provide a responsible person to operate such switch during the entire time of each race.
- (c) When the red lights are illuminated, all racing vehicles on the track shall stop as soon as possible and remain stopped until such time as the red lights are turned out.
- (d) When the amber lights are illuminated, all racing vehicles on the track shall to slow down and maintain their position unless otherwise directed to charge position by a track official.

13:62-11.4 Flagmen

- (a) Licensee's of tracks over one mile in length may use flagmen in lieu of the red and amber lights, provided the assistant flagman in the starter's stand is in constant two-way radio or telephone communication with all flagmen.

- (b) On display of the red flag all racing vehicles shall stop as soon as possible and remain stopped until such time as the red flag is removed from display.
- (c) On display of the amber flag all racing vehicles shall slow down and maintain their position unless directed to change position by a track official.

13:62-11.5 Starters

- (a) The licensee shall provide a starter(s) located within a starter's stand with an unobscured view of the entire racing surface from which to control the racing event.
- (b) At all circular or oval tracks, road courses and other locations utilizing the services of flagmen to control the event, the licensee shall also have an assistant flagman in the starter's stand. The assistant flagman used to control or start a race shall be in the starter's stand when starting and during the race.

13:62-11.6 Maximum protection

- (a) All hubrails, fences, stands and buildings shall be constructed and maintained so as to afford maximum protection for spectators.
- (b) Any spectator stand erected or relocated on or after April 1, 1960, must be located at least 25 feet from the hubrail.

13:62-11.7 Licensed facilities

Mud hop events shall only be conducted at licensed facilities.

13:62-11.8 Alcohol/controlled dangerous substances prohibited

No alcohol or controlled dangerous substances shall be permitted within the pit area. Anyone departing the pit area shall be prohibited from returning during the duration of the event if the individual has consumed alcohol or a controlled dangerous substance.

13:62-11.9 Driver requirements

- (a) Mud hop drivers shall be a minimum 18 years of age and in possess a valid driver's license from the driver's state of residence.
- (b) The driver's license number shall be recorded on an entry form and checked by the licensee for validity.
- (c) The driver shall remain with a disabled vehicle until properly escorted from the racing track.

13:62-11.10 Equipment requirements

- (a) All drivers shall wear a helmet in condition which meets or exceeds the American National Standard Institute (A.N.S.I.) Z-90.1 testing standard.
- (b) The driver of all vehicles shall wear windproof, shatterproof goggles or a face shield of the type which meets or exceeds U.S.A. standard Specifications for Head, Eye and Respiratory Protection Z2.1-1959 testing standard.
- (c) Drivers shall attired in a shirt, long pants and shoes, at a minimum. It is recommended that this clothing be of a fire retardant material.
- (d) The extension of the driver's head or arm outside the vehicle is prohibited.

13:62-11.11 Windows

- (a) The windshield of the participating vehicle may remain as originally installed.
- (b) Heavy mesh screening with a metal post covering the entire opening or plexiglass with a center post shall be utilized where the original windshield is removed.

13:62-11.12 Vehicle interior and exterior requirements

- (a) Sharp edges created by accidental vehicle contact shall be folded over to render the vehicle safe.
- (b) Alterations to the fenders or wheel wells are prohibited on stock vehicles.
- (c) All doors of participating vehicles shall remain closed during an event. Any opening shall constitute automatic disqualification.

13:62-11.13 Seat belts

- (a) Seat belts are required and shall be properly worn during the event.
- (b) A racing lap belt and shoulder harness shall be installed and utilized on any modified vehicle, provided it is:
 1. Approved through the licensee's technical inspection and found to be in good condition; and
 2. Securely affixed to the outer flooring and reinforced by a four inch by four inch steel plate and bolts of adequate tempered steel strength.
- (c) Cable, chain or straps securing devices for seat belts are prohibited.

13:62-11.14 Rollover cage

- (a) Open cab vehicles shall be equipped with rollover cage surrounding the driver of a design, construction and quality affording the driver maximum protection against injury.
- (b) Rollover bars installed in vehicle shall be a minimum of three inches above and six inches behind the driver's head.
- (c) Rollover bars shall be a minimum of 1 3/4 inch outside diameter with a wall thickness of a minimum of .09 inch.
- (d) Rollover bars welded, bolted or fastened to the flooring shall utilize a six inch by six inch by 1/4 inch base plate.
- (e) Vehicles having uni-body construction may have a rollover bar welded to the frame of the vehicle utilizing six inch by six inch by 1/4 inch base plate affixed to the outer flooring.
- (f) Rollover bars shall be plainly visible with the exception of built-in or integral rollover bars.
- (g) Owners of vehicles with built-in or integral rollover bars shall maintain and provide upon request by the Superintendent or designee the manufacturer's detail drawing establishing the dimensions and material utilized.

13:62-11.15 Bumpers

All vehicles shall be equipped with bumpers securely fastened on the front and rear. Outside bracing of bumpers shall be prohibited.

13:62-11.16 Batteries

- (a) Batteries shall be properly secured as follows:
 1. Batteries, located in the driver compartment, shall be shielded to prevent leakage in the event of damage or turnover.
 2. Batteries located adjacent to the fuel supply of the vehicle must be secured in a metal box bolted to the frame of the vehicle by at least four 3/8 inch three line bolts to apply maximum pressure against the metal box to the frame. The battery shall also have a marine box cover secured to the top of the metal box as to not allow any movement of the battery.
 3. Batteries shall not be located within the driver's compartment.
 4. Batteries located in any other area not specified above shall be shielded to prevent leakage in the event of damage or turnover.

13:62-11.17 Miscellaneous equipment requirements

- (a) The drive shaft loop shall be installed not more than 24 inches from the front and rear yokes of modified vehicles.
- (b) The radiator, if moved from the manufacturer's position, shall be shielded from the driver by a firewall.
- (c) The front and rear trunk lid shall remain securely fastened with cable or chain throughout the entire event. Any incidental opening shall disqualify the vehicle from the event.
- (d) Outer decorations on the vehicle utilizing poles, flags, staffs or other hazardous protuberances are prohibited.
- (e) Transmission and radiator cooling lines or cooling cores shall be equipped with a metal fire wall separating the driver for maximum security.
- (f) Tow hooks or tow bars shall be installed and secured to the frame of the vehicle.
- (g) No vehicle shall transport more than one person at any time during an event or warm-up.

13:62-11.18 Braking system and pedal reserve

- (a) The licensee shall test and approve each vehicle for break pedal reserve prior to the vehicle departing the pit area.
- (b) No vehicle shall be permitted to participate in any event if the braking system includes a direct application of pressure to any of the tires or any apparent deficiencies.

13:62-11.19 Automatic transmission safety mats

Any modified vehicle with an automatic transmission shall have a steel mat, plate, or blanket installed over the transmission so as to protect the driver from injury caused by the fragmentation of the automatic transmission upon explosion.

13:62-11.20 Seats

- (a) Factory installed front seats may be utilized provided it is equipped with a head rest.
- (b) Seats shall be attached to the main frame of the vehicle, the frame of the roll cage or to a substantial metal plate utilizing a minimum of six, three line, five-sixteenths inch bolts.
- (c) The base of the seat shall be installed with four five-sixteenths inch bolts not more than three inches from the outer edge at the four most practical points.
- (d) Two bolts shall be installed at the two most practical points at the top of the back of the seat and a metal strap of at least two inches in width and one-eighth inch in thickness shall connect every two bolts.

13:62-11.21 Additional track responsibilities

- (a) The licensee shall maintain safe conditions during all pit stops.
- (b) The licensee shall maintain adequate fire apparatus on location during the event, including:
 - 1. A fire vehicle that meets the National Fire Protection Association standards of a mini pumper;
 - 2. Twenty-pound fire extinguishers, at a minimum, with a minimum of 10 B.C. rating on location during an event; and
 - 3. A minimum of two protective turn out gear shall be available to track personnel.

13:62-11.22 Speed limits

All vehicles traveling in the pit area or staging area must obey a 5 miles per hour speed limit.

13:62-11.23 Licensed facilities

Mud hop events shall only take place in licensed facilities.

OWNERS, PARTNERS OR ASSOCIATES OF TRACK

Name of Applicant _____
Last First Middle
Mailing Address _____
Street Municipality
County State
Date of Birth _____ Place of birth _____ Race _____ Sex _____
Any Other Names Used _____ Social Security Number _____
Telephone:Business:() _____ Home:() _____
Relation to track : _____

Name of Applicant _____
Last First Middle
Mailing Address _____
Street Municipality
County State
Date of Birth _____ Place of birth _____ Race _____ Sex _____
Any Other Names Used _____ Social Security Number _____
Telephone:Business:() _____ Home:() _____
Relation to track : _____

Name of Applicant _____
Last First Middle
Mailing Address _____
Street Municipality
County State
Date of Birth _____ Place of birth _____ Race _____ Sex _____
Any Other Names Used _____ Social Security Number _____
Telephone:Business:() _____ Home:() _____
Relation to track : _____

Name of Applicant _____
Last First Middle
Mailing Address _____
Street Municipality
County State
Date of Birth _____ Place of birth _____ Race _____ Sex _____
Any Other Names Used _____ Social Security Number _____
Telephone:Business:() _____ Home:() _____
Relation to track : _____

BUILDING INSPECTOR'S CERTIFICATE

I, _____, building inspector of the municipality of _____
certify I have inspected the spectator stand(s) at the stated location and in my
opinion they are safe for use.

Date

Signature of Building Inspector

**STATE OF NEW JERSEY
DIVISION OF STATE POLICE
RACING CONTROL UNIT
P.O. BOX 7068
WEST TRENTON, N.J. 08628-0068**

CERTIFICATE OF INSURANCE

This is to certify that the Policy described below has been issued by the

Name of Insurance Company

to the Insured named below and is in force at this time.
It is hereby understood and agreed that this policy is non-cancelable except after thirty days written notice to the Administrator, Race Track Law, Division of State Police, Department of Law and Public Safety. P.O. Box 7068, West Trenton, New Jersey 08628-0068.

Certificate issued to:

Administrator, Race Track Law
Department of Law and Public Safety
Division of State Police

Name of Insured _____

Address _____

Policy Number	Limits of Liability	Bodily Injury
_____	\$ _____	\$ _____
	Each Person	Each Accident

Effective Date _____ Expiration Date _____

Date _____

Name of Insurance Company

Signature of Insurance Agent

Agent making certificate must be an agent as defined in N.J.S.A. 17:22-6.24.
Certificate required in accordance with N.J.S.A. Title 5:7, commonly known as the Motor Vehicle Racing Law.

NEW JERSEY STATE POLICE

TRACK REPORT:

LOCATION:

MONTHLY RACE TRACK REPORT

DATE:

MONTH:

YEAR:

DATE	TYPE OF EVENT	SPECTATOR ATTENDANCE	NUMBER OF VEHICLES PARTICIPATING	NUMBER OF ACCIDENTS WITH INJURY	UNUSUAL INCIDENTS	TOTAL NUMBER INJURIES													

EXPLANATION OF UNUSUAL INCIDENTS:

Signature of Track Representative

STATE OF NEW JERSEY
DIVISION OF STATE POLICE
AUTO RACING ACCIDENT REPORT

Date of Accident _____
Track _____
Time _____

Name of Injured Person _____ Age _____

Address _____
Last First Middle Sex

Type of Vehicle _____ Type of Event _____

Status at Time of Injury:

Driver Official Emergency Crew
 Spectator Pit-Crew Other _____
Specify

Lighting: Daylight Artificial Weather _____

Nature of Injuries _____

Treated By: _____ Time: _____ Location: _____

Explain Briefly How Accident Occurred: _____

Date Signature (Licensee or Manager)

Instructions

1. Report must be mailed to: Field Operations Section Supervisor, Division of State Police, P.O. Box 7068, West Trenton, New Jersey 08625 within 48 hours of accident.
2. All injuries occurring during race, warm up, practice, or exhibition are to be reported in accordance with the automobile racing regulations.
3. If more than one person is injured in a single accident, identification information and nature of injuries may be shown on the reverse side of this form or on separate sheets.
4. A separate form is to be used for each accident.

N.J. STATE LAW

An Act regulating the operation or conduct of motor vehicle races and exhibitions of motor vehicle driving skill and the tracks or places at which the same are operated and conducted, providing for the issuance of licenses therefor, and repealing "An act concerning motor vehicle racing and exhibitions of motor vehicle driving skill, providing for the issuance of licenses therefor, the inspection of tracks and maintenance of guard rails and fences for the protection of spectators and prescribing penalties for violations thereof," approved May twenty-seventh, one thousand nine hundred and fifty-two (P.L. 1952, c. 299).

5:7-8 LICENSE REQUIRED FEE.

No person shall operate or conduct any motor vehicle races or exhibitions of motor vehicle driving skill, or any track or other place for the holding of such races or exhibitions, unless a license to operate and conduct the same shall be first obtained from the Department of Law and Public Safety, which license said department may, in its discretion, issue to any applicant therefor upon compliance with the provisions of this act and the rules and regulations issued pursuant thereto, and the payment of a fee of one hundred dollars (\$100.00) in the manner hereinafter provided.

5:7-9 APPLICATION FOR LICENSE, FORM AND CONTENTS.

Application for a license to operate or conduct a track or other place for the holding of such races or exhibitions shall be made in writing to the department at least thirty days prior to the date on which any race or exhibition, or series of races or exhibitions, shall be held thereon, which application shall describe the track or place desired to be licensed and shall contain such further information as the department may require and such application shall be accompanied by a fee of one hundred dollars (\$100.00) of which seventy-five dollars (\$75.00) shall be returned to the applicant if the license be refused.

5:7-10 CONDITIONS FOR GRANTING LICENSE.

No license shall be issued for the operation or conduct of any such track or other place unless

(a) There is provided and maintained on the outer circumference of the track and around the entire outer circumference thereof, and in case spectators are permitted in the infield or within the inner circumference of the track, also on the inner circumference of the track and around the entire inner circumference thereof,

(1) A guard or hub rail of hard wood or other suitable material supported by posts of similar material of at least six inches in width and six inches in thickness, or round posts not less than seven inches in diameter, which are set in the ground at least four feet and shall extend above the ground at least two feet and are spaced not more than six feet apart, upon which are mounted on the side of the posts toward the track two planks of hard wood or other suitable material, at least ten inches in width by three inches in thickness, fastened on the face of the posts and running horizontally and parallel to each other and there is attached on the opposite side of the posts and not more than eight inches from the top thereof a three-quarter inch steel cable

- running around the circumference of the track and securely fastened with eyebolts to the posts or
- (2) Such other protective devices of a type or character as shall be approved by the department, after hearing in accordance herewith, and
 - (b) There is provided along any part of the track where spectators are permitted to be, whether outside of the track or in the infield, a fence of a type approved by the department, six feet in height, and located not less than four feet away from the edge of the track, and
 - (c) The applicant shall have complied with all rules and regulations made by the department under this act relating to matters of track construction and condition guard rails, pit facilities, lighting and similar safety measures.

5:7-11 EXPIRATION AND REVOCATION OF LICENSE.

Each license so issued shall expire on the thirty-first of December following the date of its issuance. Any such license may be revoked at any time, however, for failure of the licensee to comply with the requirements of this act and of the rules and regulations made thereunder relating to the issuance of licenses. A licensee shall be afforded at least three days' notice of revocation and a reasonable opportunity to be heard prior thereto. A license which has been revoked may be reinstated by the department upon compliance with the requirements of this act and payment of a reinstatement fee of twenty dollars (\$20.00).

5:7-12 CONDITIONS COMPLIED WITH.

No license shall be issued for the holding of any motor vehicle race or exhibition of motor vehicle driving skill unless the department is satisfied that all provisions of this act and of the rules and regulations made under this act governing the physical conditions surrounding or relating to such track or place have been complied with, that all gateways into the track will be provided with guard personnel sufficient in the judgment of the department for the protection of spectators, but not more than one guard shall be required for a single gate or two guards for a double gate, and that provision is made for the protection of spectators against accident by the carrying of appropriate insurance as hereinafter provided.

5:7-13 INSURANCE COVERAGE REQUIRE.

No license shall be issued for the holding of any motor vehicle race or exhibition of motor vehicle driving skill until the person applying for the license shall have filed with the department evidence satisfactory to it of the issuance of an insurance policy approved by the department and conditioned, in the case of a race or exhibition at a track or other place accommodating less than one thousand people, for the payment of not less than twenty-five thousand dollars (\$25,000.00) to any one person obtaining judgment, and not less than fifty thousand dollars (\$50,000.00) on all judgments recovered, and for the payment of not less than fifty thousand dollars (\$50,000.00) to any one person obtaining judgment, and not less than one hundred thousand dollars (\$100,000.00) on all judgments recovered, in the case of all other races or exhibitions, upon a claim or claims arising out of the same transaction or transactions, connected with the same subject of action (to be apportioned ratably among judgment creditors according to the amount of their respective judgments) for damages because of bodily injury, including death at any time resulting therefrom,

caused to any person or persons, other than the driver, and all pit area personnel as the result of an accident occurring as a result of the conduct of any motor vehicle race or exhibition of motor vehicle driving skill, for which the license is granted, under which policy the liability of the company shall become absolute when loss or damage covered by the policy occurs, and satisfaction by the insured of a final judgment for the loss or damage shall not be a condition precedent to the right or duty of said company to make payment on account of the loss or damage and which policy shall not be cancellable or annulled as to any loss or damage by an agreement between the carrier and the insured after the insured has become responsible for the loss or damage or in any other event, except on ten days prior notice to the department.

5:7-14 RULES AND REGULATIONS.

The department shall formulate and prescribe rules and regulations not inconsistent with this act, which shall be formulated in each year after holding a public hearing in the months of January or February notice whereof shall be sent by mail to each licensed track owner of the preceding year at least ten days before the hearing. Said rules and regulations shall prescribe the types or character of protective devices designed to protect participants in and spectators attending any such race or exhibition, including, but not limited to, the matters of track construction and condition, guard rails, pit facilities, lighting, inspection of vehicles and equipment, physical conditions of drivers, fire protection and medical and ambulance facilities and generally governing the conduct of all motor vehicle races and exhibitions of motor vehicle driving skill to be held within this State and governing the issuance of licenses therefor.

5:7-15 POWER TO INSPECT.

The department shall have power at all convenient times to inspect any race track or place of exhibition, licensed by it or for which application for license has been made to it, for which any license for the holding of any motor vehicle race or exhibition of motor vehicle driving skill has been applied for or issued.

5:7-16 SERVICES OF PRESENT EMPLOYEES, CIVIL SERVICE.

The department is authorized and directed, insofar as possible, to utilize the services of employees of the several divisions of the department to perform the functions and duties charged to it under this act and to appoint and remove, subject to the provisions of Title 11 of the Revised Statutes, such additional personnel necessary to assist it in the performance of the duties imposed by this act.

5:7-17 DISPOSITION OF FEES.

All fees received by the department pursuant to this act shall be paid and accounted for by it to the State Treasurer for credit to the general funds of the State.

5:7-18 FINES.

Any person, partnership, association or corporation managing, operating, or conducting a motor vehicle race or exhibition of motor vehicle driving skill, except in accordance with a license issued therefor, or violating any of the provisions of this act, shall be a disorderly person, and upon conviction shall be punished for each such offense, by a fine of not less than two hundred dollars (\$200.00) and not more than one thousand dollars (\$1,000.00) or by imprisonment for not more than one year, or by both such fine and imprisonment.

5:7-19 CHAPTER 299, LAWS 1952 REPEALED.

"An act concerning motor vehicle racing and exhibitions of motor vehicle driving skill, providing for the issuance of licenses therefor, the inspection of tracks and maintenance of guard rails and fences for the protection of spectators and prescribing penalties for violations thereof," approved May twenty-seventh, one thousand nine hundred and fifty-two, is repealed but all licenses and permits issued thereunder shall continue in full force and effect until the expiration thereof.

This act shall take effect immediately.
Approved May 29, 1953.



George Knight & Associates, Inc.

P. O. Box 55295 ♦ Tulsa, Oklahoma 74155 ♦ (918) 258-4745



MUD BOG/MUD DRAG SPECIFICATIONS

I. COURSE REQUIREMENTS

1. Maximum length of Mud Pit: Indoor - 160 Ft.
Outdoor - 250 Ft.
2. Width optional - See diagram attached for minimum distance requirements.
3. Minimum Depth 36 Inches. Vertical wall on both sides must be pitched at 90 Degrees with at least 10 inches exposed above bog level and must be parallel with bog.
4. Barrier must be erected on each side of bog, minimum 36 inches high from start to finish. Barrier must be concrete or double stran hiway steel or D.O.T. Barrier. If concrete, sections must be pinned together with satisfactory reinforcing rods.
5. Shutdown area must be equal to or greater than the distance of the timing zone, and must be measured from the finish line.
6. One third of the finish line must be in the bog.
7. On all indoor events, the remainder of the shutdown area must be covered with a minimum of three (3) inches (depth) of sand.
8. Pit area may be to either side of competition area behind spectator area or behind the start line. If pits are on one side and spectators on the other, the same fencing is required on both sides.

II. EQUIPMENT REQUIREMENTS:

1. No paddle or scoop tires, or variations of same, will be allowed. Use of such will invalidate insurance.
2. No exotic fuels, such as propylene oxide, nitrouse oxide or nitro methane will be permitted.
3. No vehicle will be allowed to compete that weighs less than 1800 pounds.

PAGE 2
(Equipment Requirements)

4. Push type kill switches, tested PRIOR to each run, practice or competition, will be mounted on each competition vehicle within easy reach of the driver when belted in.

III. GENERAL INFORMATION:

1. Absolutely no person, including officials, timers, photographers, etc., will be permitted beyond the finish line during competition runs.
2. All competition will be conducted from a STANDING START. Rolling starts of any kind will not be tolerated and violations of this can result in your coverage being suspended at that point.
3. Any person entering the pit area must be required to sign a valid Release. Spectators are NOT allowed in the Pit Area during competition. They may enter before or after competition has started but only to view a "static" display of equipment. Loading or unloading of equipment will be considered to be during competition.
4. Any person injured in the pit area during competition for whom you do not have a properly signed release will be subject to a Deductible of \$25,000 for any loss paid.
5. Minimum age for ALL participants, including pit crew, is 18.
NO EXCEPTIONS!!!!

* IV. SPECTATOR AREA:

1. Spectator areas can be on either or both sides of bog.
2. Crowd control fence must be a minimum of 30 feet from the pit barrier and parallel to same.
3. Acceptable crowd control fence may be chain link, woven wire or welded wire. Supporting poles may be up to 10' apart but must be anchored in the ground.
4. If event is held in a rodeo arena or a race track currently insured, it is permissible to allow spectators to sit in the stands where they would normally sit. However, no person will be allowed to sit beyond the finish line of the competition zone.

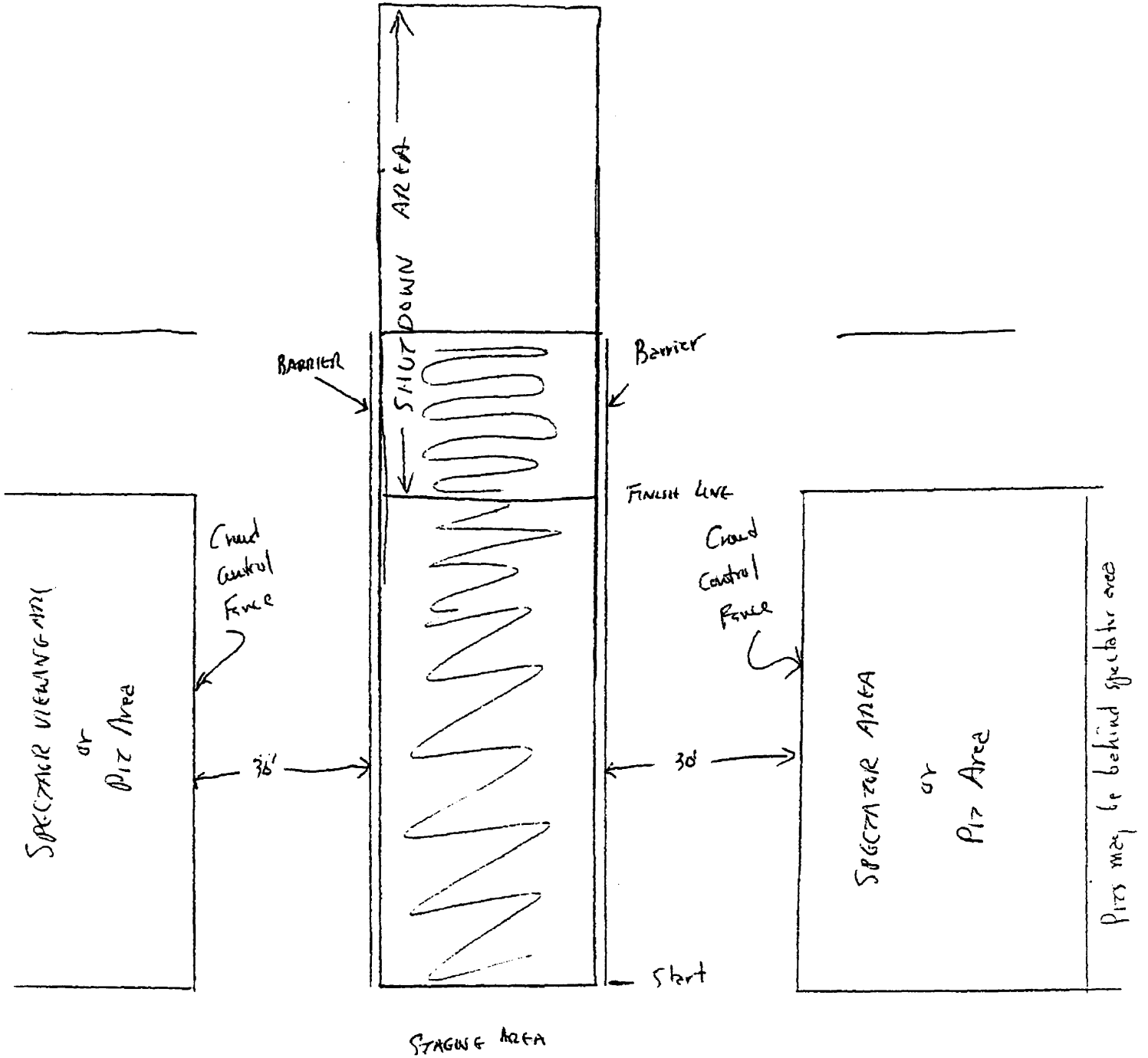


MUD BOG - DRAG

1. NAMED INSURED: _____
2. MAILING ADDRESS: _____
3. LOCATION OF BOG: _____
4. INDOOR: _____ LENGTH OF COMPETITION RUN: _____
 OUTDOOR: _____ LENGTH OF COMPETITION RUN: _____
5. INDOOR _____ LENGTH OF SHUTDOWN AREA: _____
 OUTDOOR: _____ LENGTH OF SHUTDOWN AREA: _____
6. INDOOR: _____ LENGTH OF BOG AREA: _____
 OUTDOOR: _____ LENGTH OF BOG AREA: _____
7. PREVIOUS INSURANCE CARRIER: _____
8. PREMIUM: 3 PREVIOUS YEARS: _____
9. LOSSES: 3 PREVIOUS YEARS: _____
10. LIMIT REQUIRED: \$300,000 _____ \$500,000 _____ \$1,000,000 _____
11. EFFECTIVE DATE: _____ # OF EVENTS: _____
12. TYPE VEHICLES: _____
13. PERSON TO CONTACT FOR INFORMATION: _____
 ADDRESS: _____ PHONE: _____ - _____ - _____
14. BLEACHERS: HT: _____ CONSTRUCTION: _____
 SAFETY RAILS ON SIDES & BACK: _____
15. ATTACH A COMPLETE DIAGRAM WITH DETAILS OF SPECTATOR FENCING AND ALL BARRIERS, GIVING CONSTRUCTION, HEIGHT & DISTANCES FROM TRACK AND SPECTATOR AREAS.

ATTACH PHOTO'S OF BOG, GUARD RAILS, SPECTATOR FENCING AND AT LEAST ONE SHOT FROM THE START LINE TO THE FINISH LINE.

ALL QUESTIONS MUST BE ANSWERED OR APP WILL BE RETURNED. THE COMPLETION OF THIS FORM IS NOT AN OFFER OF INSURANCE AND NO COVER CAN BE AFFORDED UNTIL NOTIFIED IN WRITING BY GKA, INC.





1712 Magnavox Way
P.O. Box 2338
Fort Wayne, IN 46801
(219) 459-5000 Fax (219) 459-5866

GENERAL APPLICATION

APPLICANT INFORMATION

Name of Insured (as will appear on policy):
Doing business as:
Mailing address:
City: State: Zip: Phone:
Contact person:
Person is: Owner Promoter Agent Other
Day phone: Night phone: Fax#:
Name of Agency/Brokerage:
Contact person:
Mailing Address:
City: State: Zip: Phone: Fax#: Tax Id:

- 1. Nature of business/description of operations/event:
2. Insured is: Corporation Partnership Joint Venture Other (explain):
3. In what state is the organization headquartered/chartered?
4. Policy period being requested: From To
5. Estimated number of events:

COVERAGE INFORMATION

Check the type of coverage and indicate the limits desired:

General Liability Primary Excess Participant Legal Liability AD&D Primary Medical Excess Medical Weekly Indemnity Catastrophe Medical Property Casualty Property Inland Marine Auto
Other:

PRIOR CARRIER INFORMATION (NEW BUSINESS ONLY)

Table with 6 columns: YEAR, PREVIOUS AGENT, COMPANY, LIABILITY LIMITS, PREMIUM, LOSSES

PLEASE SUBMIT A COPY OF PREVIOUS/PRESENT POLICY(S)

**THIS DISCLAIMER/SIGNATURE PAGE
APPLIES TO THE FOLLOWING:**

FORM NUMBER	DESCRIPTION
#SL 38	GENERAL APPLICATION

K&K INSURANCE GROUP, INC. for the Insuring Company, shall be permitted but not obligated to inspect the INSURED'S property and operations for UNDERWRITING AND/OR LOSS CONTROL PURPOSES at any time. Neither the right to make an UNDERWRITING AND/OR LOSS CONTROL INSPECTION nor the making thereof nor any report thereof shall constitute an undertaking, on behalf of or for the benefit of any insured, or others, to forecast any accident or its severity or determine or warrant that such property or operations are safe or healthful, or are in compliance with any engineering standards, rules or regulations. The establishment of underwriting criteria. UNDERWRITING AND/OR LOSS CONTROL INSPECTIONS ARE FOR THE SOLE PURPOSE OF DETERMINING THE INSURABILITY OF CERTAIN PROPERTY AND OPERATIONS, underwriting, and seeking to reduce claims against insurance and are not for the benefit of any insured or third party. The insured is solely responsible for the safety of its property and operations and shall not rely upon any Underwriting And/Or Loss Control Inspections or activities to determine the safety of its property of operations and shall not diminish or forego its own safety practices and procedures.

I UNDERSTAND THAT THIS APPLICATION AND ALL INFORMATION SUPPLIED IS PART OF THE APPLICATION PROCESS AND WILL BE RELIED UPON BY THE INSURANCE COMPANY IN DETERMINING WHETHER TO PROVIDE THE INSURANCE COVERAGE HEREIN REQUESTED AND THAT THE APPLICATION WILL BECOME A PART OF ANY CONTRACT OF INSURANCE ENTERED INTO. ANY MATERIAL MISREPRESENTATION OR FALSE STATEMENT MAY ENTITLE THE INSURANCE COMPANY TO RESCIND THE POLICY, VOIDING ALL INSURANCE COVERAGE. I HEREBY WARRANT, REPRESENT AND CONFIRM THAT I HAVE READ ALL OF THE QUESTIONS AND ANSWERS ON THIS APPLICATION AND THAT, TO THE BEST OF MY KNOWLEDGE, ALL INFORMATION PROVIDED IN THIS APPLICATION IS COMPLETE, TRUE AND CORRECT. I FURTHER WARRANT THAT I HAVE MADE OR WILL MAKE THE NECESSARY MAINTENANCE INSPECTIONS AND THAT ALL NECESSARY REPAIRS HAVE BEEN MADE TO ENSURE THAT MY PROPERTY AND OPERATIONS ARE AND WILL REMAIN IN COMPLIANCE WITH ANY UNDERWRITING CRITERIA FURNISHED ME.

IT IS UNDERSTOOD AND AGREED THAT NO INSURANCE IS IN EFFECT UNTIL THIS APPLICATION IS ACCEPTED BY THE COMPANY OR COMPANIES IN WRITING.

DATE	SIGNATURE	TITLE

K&K, for the insuring company, may survey the insured's property and operations for underwriting purposes and conduct loss control activities and make recommendations to reduce insurance claims at any time. Such activity, when engaged in, shall not constitute an undertaking, on behalf of or for the benefit of any insured or others, to determine or assure that such property or operations are safe or healthful, or are in compliance with any engineering standards, rules, or regulations. Underwriting qualifications, recommendations, surveys, and loss control activities and recommendations are only for purposes of determining insurability and reducing claims against insurance are not for safety purposes. The insured is solely responsible for the safety of its premises and operations and shall not rely upon any surveys or recommendations to determine or assure the safety of its facilities or operations and shall not diminish or forego its own safety practices and procedures.



1712 Magnavox Way
P.O. Box 2338
Fort Wayne, IN 46801
(219) 459-5000 Fax (219) 459-5866

APPLICATION FOR DRAG RACING INSURANCE

GENERAL APPLICATION MUST ALSO BE COMPLETED WITH THIS APPLICATION

Type or print legibly for correct policy issuance. Answer ALL applicable questions.

Indicate questions not applicable with "N/A."

APPLICATION MUST BE SIGNED

1. Insured

- a. Track name _____ Account Code _____
- b. Track Address / Location _____
_____ Phone () _____
- Strip Length _____ Shut Down Length _____
- c. Insurance coverage is requested for _____ Weekly Events _____ Specials Only _____ Including Premises Liability
- d. Number of events anticipated that will involve _____ Carbtorated or injected gas or alcohol _____ Blown alcohol
_____ Blown nitro methane (more than 8 competition vehicles) _____ Jets _____ Cycles only
- e. Are you planning any events other than drag racing? _____ Yes _____ No If yes, list _____

2. Additional Insureds / Vested Interest

Name	Relationship To Insured
a. _____	_____
b. _____	_____

3. Maximum number of attendance for a single day _____

4. Total annual attendance (estimated) _____

5. Schedule or Calendar of Events (please remit copy with application)

6. Security and Emergency Plan

- a. Number and type of security personnel _____ Professional Service _____ Uniformed Officers _____ On-Duty
_____ Off-Duty _____ Employees
- b. Is there a **contracted / state-certified** ambulance and two EMT/paramedics on-site during events? _____ Yes _____ No
If no, explain in detail _____
- c. Distance to nearest emergency center _____ Hospital _____
- d. Number, type and size of fire extinguishers on-site during events _____

- e. How frequently are fire extinguishers checked? _____ By whom? _____
- f. Distance to nearest fire station _____ Professional _____ Volunteer
- g. Are ALL areas on premises available to spectators and/or participants inspected periodically for slip, trip or fall obstacles?
_____ Yes _____ No If yes, how frequently _____

h. Is facility in compliance with all known township, city, county, state and/or federal building, seating, concession and sanitation codes? Yes No If no, explain _____

7. Track Management

YES

NO

a. Are qualified race vehicle tech. inspectors provided? _____

b. Are drivers under the age of 16 permitted? _____

c. Are persons under the age of 14 permitted in restricted areas? _____

d. Are paid spectators permitted to enter pits/restricted areas during competition? _____

Staging lanes? _____

e. Are spectators or non-competing participants permitted beyond the end of the timing zone? _____

f. Are approved helmets and approved restraint belts required? _____

g. Is the K&K approved waiver and release form EXCLUSIVELY used for all persons permitted in restricted areas? _____

8. Customer Services

a. Are alcoholic beverages permitted on premises? _____

b. Are alcoholic beverages sold on premises? _____

g. Are signs posted in high traffic areas or announcements made indicating assumptions of risk? _____

9. Coverage At Other Locations

Will you require coverage at other locations? _____

If yes, list information (endorsement required).

I UNDERSTAND THAT THIS APPLICATION AND ALL INFORMATION SUPPLIED IS PART OF THE APPLICATION PROCESS AND WILL BE RELIED UPON BY THE INSURANCE COMPANY IN DETERMINING WHETHER TO PROVIDE THE INSURANCE COVERAGE HEREIN REQUESTED AND THAT THE APPLICATION WILL BECOME A PART OF ANY CONTRACT OF INSURANCE ENTERED INTO. ANY MATERIAL MISREPRESENTATION OR FALSE STATEMENT MAY ENTITLE THE INSURANCE COMPANY TO RESCIND THE POLICY, VOIDING ALL INSURANCE COVERAGE. I HEREBY WARRANT, REPRESENT AND CONFIRM THAT I HAVE READ ALL OF THE QUESTIONS AND ANSWERS ON THIS APPLICATION AND THAT, TO THE BEST OF MY KNOWLEDGE, ALL INFORMATION PROVIDED IN THIS APPLICATION IS COMPLETE, TRUE AND CORRECT. I FURTHER WARRANT THAT I HAVE MADE OR WILL MAKE THE NECESSARY MAINTENANCE INSPECTIONS AND THAT ALL NECESSARY REPAIRS HAVE BEEN MADE TO ENSURE THAT MY PROPERTY AND OPERATIONS ARE AND WILL REMAIN IN COMPLIANCE WITH ANY UNDERWRITING CRITERIA FURNISHED ME.

IT IS UNDERSTOOD AND AGREED THAT NO INSURANCE IS IN EFFECT UNTIL THIS APPLICATION IS ACCEPTED BY THE COMPANY OR COMPANIES IN WRITING.

Signed _____

Date _____



MUD BOG UNDERWRITING QUALIFICATIONS

A. Course Dimensions:

1. Maximum length of the bog is 150 feet for indoor events and 250 feet for outdoor events.
2. Minimum bog/pit depth is 36". There must be a vertical 90 degree wall with at least 10 inches high exposed above the bog level parallel with the bog. Bog width optional.
3. Positive barrier must be a minimum of 36 inches high, parallel to the sides of the course and adjacent to both sides of the course. (Acceptable positive barrier can be concrete, doublestrand highway steel, or D.O.T. barrier. If concrete barrier sections are used, sections must be pinned together with minimum 3/4" OD reinforcing rod.)
4. Shutdown length must be equal to the distance of the timing zone, measured from the finish line.
 - a. A minimum of 25% of the shutdown must be in the bog.
 - b. On indoor events, the remaining length of the shutdown (the portion out of the bog) must be covered with sand to assist in slowing the vehicles.
5. A disqualification line must be established in the shutdown area a minimum of 40 feet from the end barrier.
 - a. A trap zone, a minimum of 40 feet in length, must be used beginning at the disqualification line to the positive barrier at the end of the course. Trap material must be in volume and consistency sufficient to trap the fastest and heaviest vehicle, such as use of pea gravel outdoors and use of sand indoors. (Banded tire walls may alternatively be used as a buffer in front of the positive barrier in lieu of a sand/gravel trap.)
6. A final positive barrier, consisting of like material as in item #3, must be across the entire end of the trap zone.
7. Crowd control fence must be 30 feet behind and parallel to all positive barrier on sides and between all areas spectators and participants are permitted to view from. (Acceptable crowd control fencing can be chain link, woven wire, welded wire or of a nylon/plastic material. Supporting posts or poles may be a maximum of 10 feet apart and attached to fencing. Posts or poles are to be anchored in the ground.)

B. Spectator Area:

1. If event is held at an insured oval track, the event may held so that the spectators remain in the same specifically designed spectator area that is normally used for track events, unless a bog area was specifically designed and accepted by K & K with the same or better than above protection standards.

C. Equipment Requirements:

1. No paddle or scoop tires, or variations of same, are permitted.
2. No nitro methane, nitrous oxide or propylene oxide.
3. Blower systems must meet NHRA standards.
4. 1,800 pound minimum competition vehicle weight.
5. Four (4) point lap and shoulder belts required.
6. D.O.T. or Snell '80 approved helmets are mandatory.
7. Push type kill switches must be on all competing vehicles and be within easy reach of the driver when belted in. Kill switches must be tested for functionality before each event.

D. Competition Requirements:

1. All runs will begin from the start line from a standing start (no rolling starts).
2. The K & K Waiver and Release form and system will be strictly enforced for all participants in restricted areas; no exceptions.
 - a. A \$25,000 deductible for participant claims with no signed waiver and release form due to inadvertent error will apply.
3. A fully earned minimum and deposit liability premium of \$10,000 will be required at inception. Application per event premiums will be applied against the deposit as events are pre-reported.
4. Standard fire protection must be on-site for all events.
5. A transporting ambulance and two EMTs (or their equivalent) must be on-site for all events.
6. Photographers must be behind barrier system.
7. No persons, including photographers, permitted at the end of and in line with the finish line and shutdown area.
8. Pits must be self contained and no spectators are permitted in the pits during competition.
 - a. Spectators may be permitted in the pits for static display of vehicles only and only before or after competition is taking place.
9. Participant Accident insurance and Participant Legal Liability is optionally available in conjunction with Spectator Liability. Participant Accident must be purchased with and to validate Participant Legal Liability.

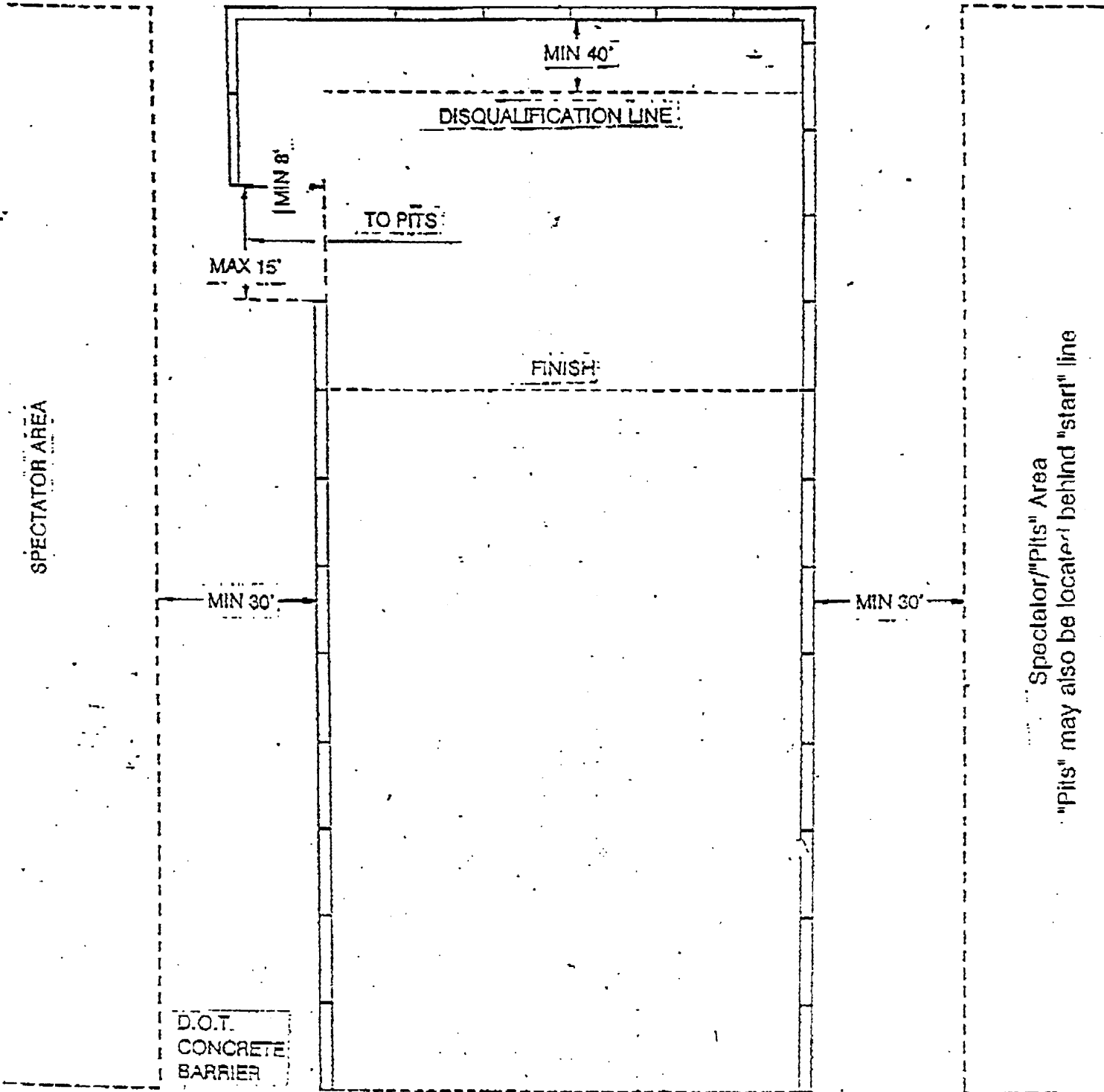
(Date)

(Signature)

K & K , for the insuring company, shall be permitted but not obligated to inspect the Insured's property and operations for underwriting purposes at any time. Neither the right to make an underwriting inspection nor the making thereof nor any report thereon shall constitute an undertaking, on behalf of or for the benefit of any Insured, or others, to forecast any accident or its severity or determine or warrant that such property or operations are safe or healthful or are in compliance with any engineering standards, rules or regulations. Underwriting inspections are for the sole purpose of determining the insurability of certain property and operations and not safety. The Insured is solely responsible for the safety of its facilities and operations and shall not rely upon any underwriting inspections to determine the safety of its track or operations and shall not diminish or forego its own safety practices and procedures.

CROWD CONTROL FENCE

D.O.T. CONCRETE BARRIER



FACILITIES FOR MUD BOG EVENTS

